

## 8.0 The masterplan

The indicative masterplan (opposite) represents the council's interpretation of how the key components of this document may be realised in a spatial form across the SPD area. The spatial masterplan recognises the key objectives of the project, takes into consideration primary constraints and adopts the development principles identified in section 7.0. In doing so, the spatial masterplan offers a clear indication of how a strong urban design agenda and sound design principles can be realised across the area.

The primary concept around the spatial masterplan focuses heavily around delivering a vibrant new city quarter which harnesses the unique heritage and physical assets of the area. Central to this concept is the delivery of a high quality scheme which celebrates the setting of the cWHS and reinforces its historic relationship with the river through the highest possible standards in urban design to create an attractive and distinctive visitor destination.

The resulting indicative masterplan represents a scheme which makes the most efficient use of the space available. It offers both a high quality but realistic design solution which balances the sensitive needs of the cWHS with the needs of promoting a viable solution to bringing forward development of the large swathes of brownfield and redundant land which currently detracts from the quality of the cWHS, the river corridor and the wider area.

The spatial masterplan is not prescriptive in limiting how the site must be developed, nor is it exhaustive in suggesting this as the only way the site can be redeveloped. It is an interpretation of how the council's aspirations for the area can be accommodated within the physical and spatial constraints identified in this document.

Other interpretations of how the site could be taken forward to meet the objectives of the masterplan may be appropriate, and the council will be open to discussions with any interested parties who wish to provide their own interpretation of the masterplan. However, any proposals which compromise the viability, deliverability and quality of any part of this SPD will be resisted. To further strengthen this approach and to supplement the spatial masterplanning process, the following Design Code has been developed to provide further clarity on the expectations of the council for the redevelopment of the area.

### Potential uses

The masterplan is based around the delivery of mixed use development capable of providing a major quarter to the city. However, to ensure uses within the masterplan are compliant with this aspiration and accord with the objectives of this document, all uses as identified within the Use Class Order 2010 are categorised below.

Uses considered to be appropriate in certain locations across the masterplan area and which will be encouraged include:

- Cafes, bars and restaurants (Use classes A3 and A4)
- Business uses including general offices and research and development (Use class B1)
- Hotel uses (Use class C1)
- Residential dwellings including apartments (Use class C3)
- Non-residential institutions including education, crèches, galleries and museums (Use class D1)

Ancillary uses considered to be appropriate in certain locations across the masterplan area but which will be subject to stricter control include:

- General retail uses (Use classes A1- A2)
- Hot food takeaways (Use class A5)
- Residential institutions (Use class C2)
- Leisure uses (Use class D2)

Uses considered to be inappropriate for the masterplan area and which will be resisted include:

- General industrial uses (Use class B2)
- Storage and distribution uses (Use class B8)
- Secure residential institution (Use class C2A)
- Housing in multiple occupation (Use class C4)

# Wearmouth masterplan

**Candidate World Heritage Site**  
Incl. St Peter's Church and  
associated grounds

**Business Quarter**  
Incl. Site of North Sands  
Business Park

National Glass  
Centre

**Learning Quarter**  
Incl. Sunderland University  
St Peter's Campus extension

**Residential Quarter**  
Incl. Bonnersfield development  
site and land along North Quay

Dame Dorothy Street

Wearmouth Bridge

Fish Quay

**South Shore Development Sites**  
Incl. Scotia Quay, Numbers Garth  
and High Street East development sites

Echo 24

West Wear Street

High Street East





## 9.0 Design code

The Wearmouth Masterplan is a large scale masterplan which not only promotes the highest possible design qualities, but also represents a complex scheme involving a number of valuable heritage assets, likely to be delivered over a large time period. Due to the likely timescale of implementation, varying development sensitivities, and the varied land ownership issues across the area, the council has identified the area as appropriate for the use of a Design Code in line with advice contained within the Department for Communities and Local Government's (DCLG) 'Preparing Design Codes – A Practice Manual', and the Commission for Architecture and the Built Environment (CABE) 'The use of urban design codes'.

The Design Code sets out the standards of urban design the City Council expects to be delivered through the redevelopment of the masterplan area, and is a document which should be used by developers, architects and designers in advance of any planning submission being made. The Design Code has been drawn up in accordance with existing national and local planning policy and guidance – namely the council's adopted Central Area Urban Design Strategy (CAUDS) SPD, Residential Design Guide SPD and CABE's Building for Life assessment criteria – and should be read in conjunction with these documents. The council will

use these documents and the Design Code to review, evaluate and determine planning applications for the site. It is therefore imperative that any planning submissions should be accompanied by a Design and Access statement which demonstrates compliance of the proposal with the code.

The Design Code is a tool which should be used to supplement and add clarity to the Wearmouth Masterplan. Both the masterplan and the code complement each other: the masterplan offering a spatial interpretation on how the site should be developed, and the code offering a greater level of detail to the urban design principles guiding the masterplan. It is therefore crucial that the masterplan and code are read as a single entity helping guide and inform proposals for the site.

The structure of the Design Code breaks the masterplan down into a number of components. It is widely accepted that high quality urban design is not purely about the appearance of buildings, but is also heavily influenced by the relationship between buildings, public realm, streets and squares. The following structure of the code has therefore been developed to identify the key components the council considers most important when developing the masterplan:

1. Urban structures and character areas
2. Land uses and flexible space
3. Views and vistas
4. Block principles
5. Building heights and densities
6. Gateways and landmarks
7. Building types and frontages
8. Street types
9. Access and servicing
10. Landscape and public realm

In order to provide an understanding of the guidance, a description is provided of the rationale behind each component of the code. This is then followed by a list of specific codes or criteria – often in the form of bullet points – which should be addressed and acknowledged by the applicant as part of any submission.

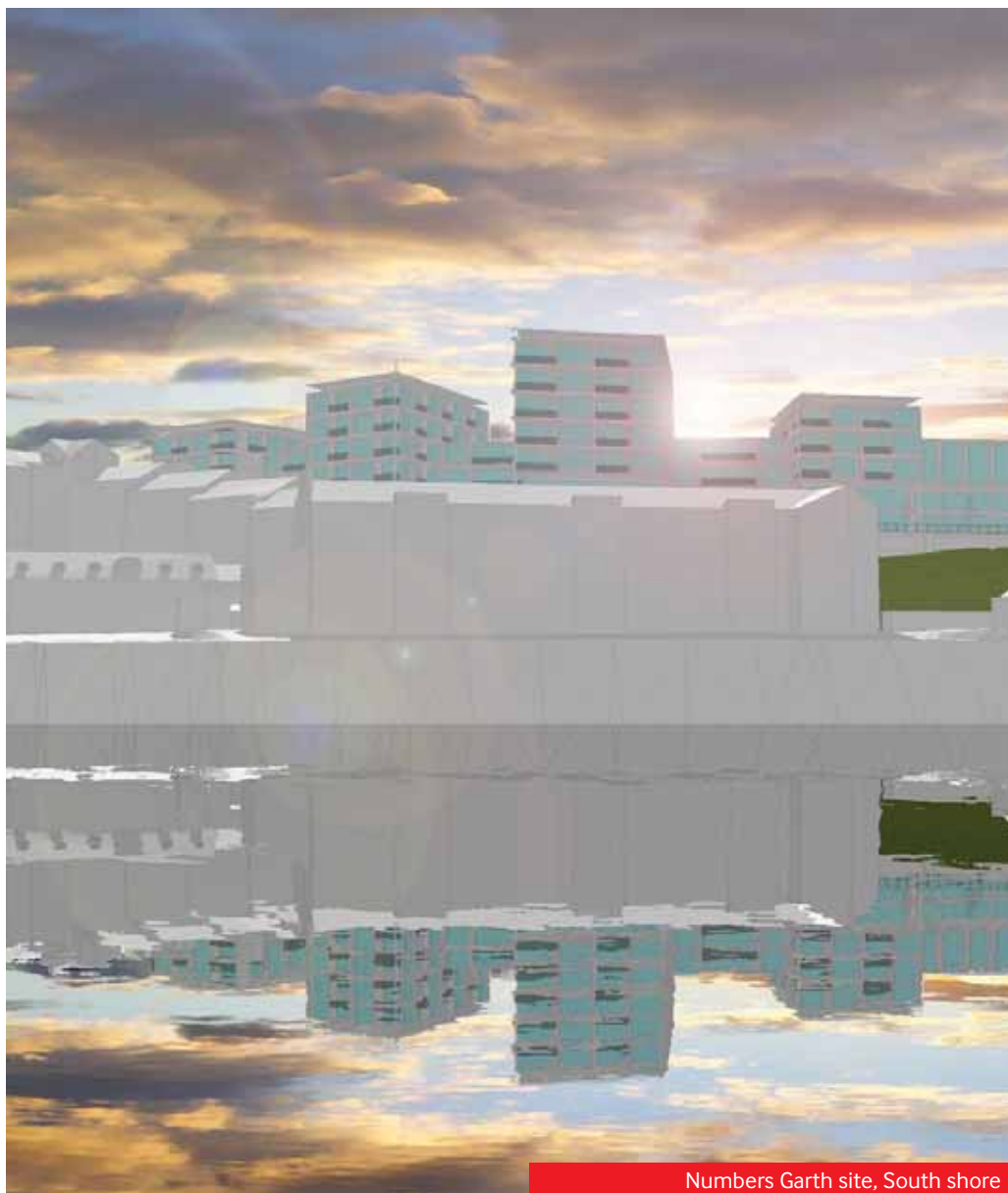
All illustrations and drawings within the code are a demonstration of how the code could be realised, and are for illustrative purposes only.

A copy of all other nationally and locally adopted planning and urban design policies which should be read in conjunction with this Design Code can be found at: [www.sunderland.gov.uk/urbandesign](http://www.sunderland.gov.uk/urbandesign)



View of St Peter's Church





Numbers Garth site, South shore

## 9.1 Urban structure and character areas

The Wearmouth Masterplan is based on a structure of new and existing developments across the area, focusing development around the cWHS and along the river to provide a vibrant and integrated new urban quarter to the city.

The individual design components which make up the masterplan such as gateways, nodes, building heights and densities and spaces have been analysed and distributed across the area to locations which best suit their roles. The consequence of this is a clearly defined, structured and legible masterplan made up of distinct character areas which have their own identity and role according to their location and setting within the masterplan area. These are to be bound together by a network of attractive and safe pedestrian and multi-user routes spanning the area, improving links between the cWHS and the river and addressing issues regarding site permeability.

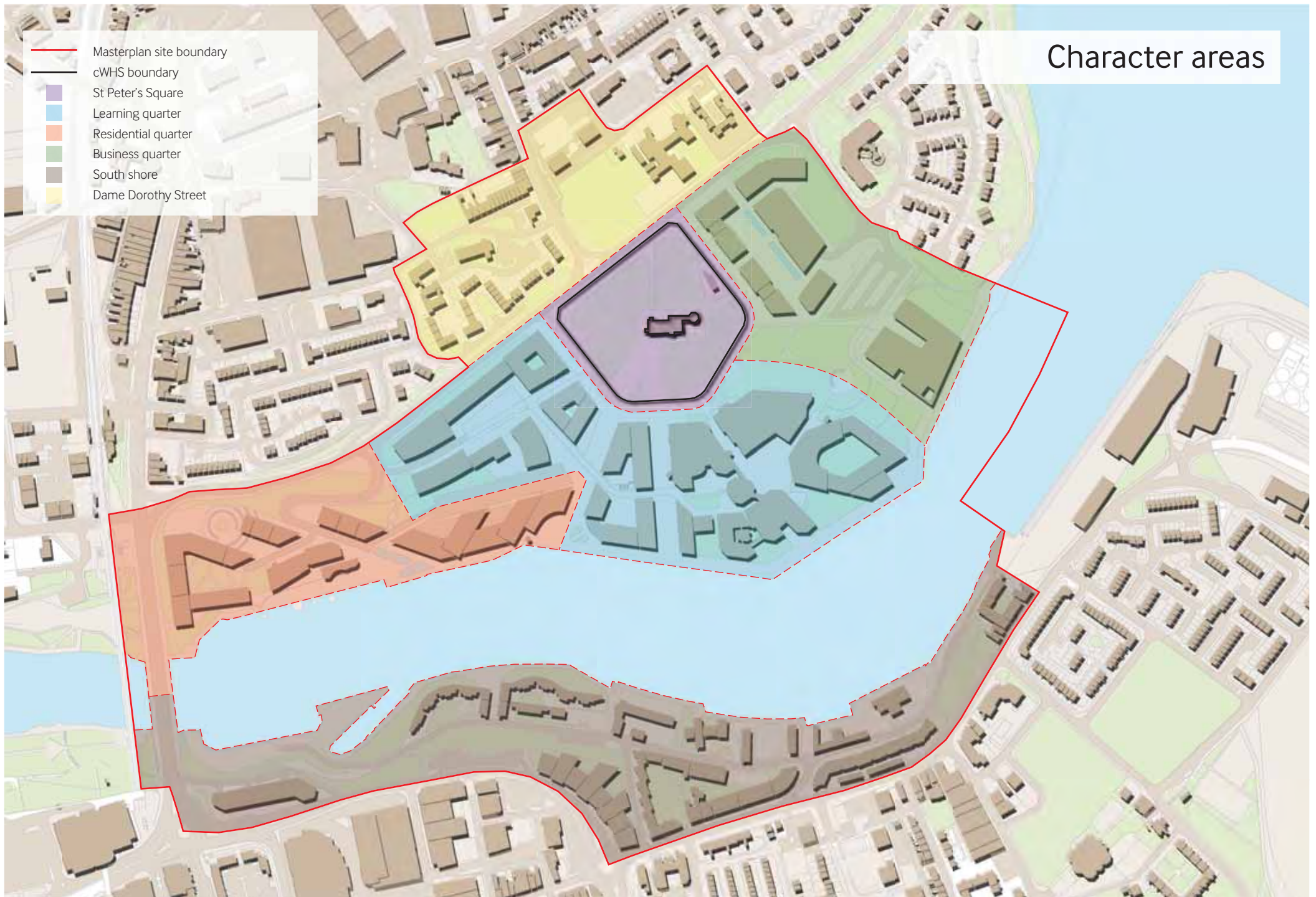
This will allow for the delivery of a major regeneration project which is capable of evoking its own clear identity, capable of providing a recognisable brand for the cWHS, the river corridor, and the wider City of Sunderland. The individual character areas will allow the area to be broken down into distinct quarters with their own unique identity and personality. This ensures that development seamlessly fits and integrates together as a comprehensive development, without becoming either dominant or monotonous in their appearance, but changing and evolving when passing from one end of the masterplan area to the other.

The masterplan consists of six distinct character areas. The vision behind these is to establish a coherent urban hierarchy as described above, ranging from the more intensive commercial and residential quarters towards the river, and the less intrusive development around the cWHS and the conservation area covering the south shore.

The Wearmouth Masterplan therefore consists of the following character areas which can be seen on the plan opposite:

- St Peter's Square
- Learning quarter
- Residential quarter
- Business quarter
- South shore
- Dame Dorothy Street

## Character areas





### St Peter's Square

- The St Peter's Square character area is located at the heart of the masterplan area and covers the allocated cWHS itself as identified under UDP Alteration No.2 Policy NA28.A. Consequently, the area is defined by Dame Dorothy Street to the north, and St Peter's Way to the west, south and east.
- This character area forms the heart of the wider masterplan area, providing a formal green open space on which pedestrian movement and sightlines will be focused. This will be the primary area of public open space within the entire masterplan area.
- At the centre of the space will be the St Peter's Church building and the monastic building remains which have been located to the south of the existing building. These form the key elements of the cWHS bid, and consequently should be celebrated and protected as a matter of priority.
- Activity within this character area will be provided as a consequence of surrounding development plots opening out and pushing activity towards the square. No building work itself will be permitted within the St Peter's Square character area unless in extreme circumstances where it can be demonstrated such works are required to ensure the future security of either the church building or monastic remains.



cWHS, St Peter's Square

- The dominance of roads within the character area, namely St Peter's Way will be significantly reduced in order to accommodate the anticipated increase in pedestrian activity as a consequence of WHS inscription and surrounding development. This will be focused around the downgrading of St Peter's Way to a pedestrian priority and shared surface street offering 'access only' to vehicular traffic.
- The public realm in the area will consist primarily of formal hard and soft feature landscaping used to celebrate the area's rich heritage. This will be implemented through the St Peter's Landscape Strategy, the initial phases of which are being implemented during the writing of this document.

### Learning quarter

- The learning quarter consists of land between the cWHS and the river, and extends up to the National Glass Centre in the east, includes the existing St Peter's University campus, and encompasses a large area of land to the west which is envisaged to be used for an extension of the St Peter's Campus in the future. This is one of the most important character areas in the masterplan and is likely to experience one of the highest level of building works.
- This area will accommodate the crucial pedestrian and visual links thought to be imperative in enhancing the setting of the cWHS through accommodating protected views of the church, and reinforcing the historic links between the church and the River Wear.

- The learning quarter will therefore play a crucial role in acting as a primary pedestrian gateway to those wishing to enter the cWHS from the river shore. The area therefore needs to be safe, attractive and vibrant with street cafes and restaurants spilling into the public realm creating a distinctive cosmopolitan character capable of providing a memorable first impression to visitors. The area will also act as a vehicular gateway to those entering the cWHS area off Dame Dorothy Street along Charles Street.
- Activity in the area will be focused primarily along three frontages including the frontage bounding the cWHS, the frontage facing on to the river shore, and finally the frontages bounding the key pedestrian routes and view corridors linking the cWHS with the river. Notwithstanding this, other important and highly prominent frontages include those bounding Charles Street and Dame Dorothy Street to the north.
- Uses considered appropriate and to be encouraged in this area include:
  - ▣ Food and drink uses including cafes, bars and restaurants (Use classes A3 and A4)
  - ▣ Non-residential institutions including education, galleries and museums (Use class D2)
  - ▣ Ancillary retail uses (Use class A1)
- Uses considered permissible with justification in this area include:
  - ▣ Business uses including general offices and research and development (Use class B1 only)
  - ▣ Hotel uses (Use class C1)
  - ▣ Student accommodation (Use class C3)
- The scale and massing of buildings within this area will consist largely of buildings between 4 and 5 storeys in height depending on their location and setting. Building heights can be increased towards the river away from the cWHS where increases in building heights can be accommodated within falling site levels.
- Additional floors may be permissible at key gateways and nodes where it can be demonstrated the increase in height will not have a detrimental impact on the setting of the cWHS, although no building in such locations will be permitted to exceed a height of 6 storeys (21 metres)
- It should be a priority to ensure all ground floor uses are active and offer a positive relationship with the public realm. Upper storeys of buildings should enable and maximise views across the cWHS and along the river corridor to increase levels of natural surveillance. Features such as building terraces and balconies will therefore be encouraged.
- Building blocks should be serviced internally to minimise the need for service entrances and areas to the front of the building being accessed directly off areas of public realm.
- The dominance of roads which currently exist within the area (namely Charles Street) will be reduced, and greater priority given to pedestrians to improve pedestrian links between the cWHS and the river.
- The public realm in the learning quarter will consist primarily of hard formal, high quality landscaping. Both the river shore and the routes linking the cWHS and the river will comprise of high quality materials, lighting, street furniture and tree planting to provide a safe, attractive and vibrant area of public realm.



Views of St Peter's Church through the learning quarter



### Residential quarter

- The residential quarter is located in the north west section of the masterplan area and is defined by Dame Dorothy Street to the north, the Wearmouth Bridge to the west, the river to the south, and the learning quarter to the east. This character area currently accounts for the largest number of potential development sites and is one of the most prominent areas within the masterplan due to its proximity to the Wearmouth Bridges.

- The quarter's setting is also one of the most challenging, as the area's steep topography will need to be addressed if the area is to come forward for development. Issues associated with the busy Dame Dorothy Street and its impact on severing pedestrian links between the St Peter's Metro Station immediately to the west of the site will also need to be addressed if the masterplan is going to succeed in attracting greater pedestrian footfall down to the river along to the cWHS.
- The residential quarter will therefore play a crucial role as a primary pedestrian gateway into the masterplan area as well as acting as a major gateway into the wider city centre due to its prominent setting. High quality public spaces, sensitively located statement architecture, and safe and attractive streetscapes will therefore play a crucial role if the area is to be able to draw pedestrian flows down to the riverside and along to the cWHS as well as celebrating its setting adjacent to the iconic Wearmouth Bridge.
- Activity within the area will be focused largely in three areas - around a gateway space to be located in the north west corner of the character area; along the river shore; and bounding routes linking these two areas to ensure routes down to the river are as vibrant, active and legible as possible.
- Uses considered appropriate and to be encouraged in this area include:
  - ▣ Residential accommodation (Use class C3)
  - ▣ Student accommodation (Use class C3)
  - ▣ Hotel uses (Use class C1)
  - ▣ Food and drink uses including cafes, bars and restaurants (Use classes A3 and A4)
  - ▣ Ancillary retail uses (Use class A1)
- Uses considered permissible with justification in this area include:
  - ▣ Business uses including general offices and research and development (Use class B1 only)
  - ▣ Non-residential institutions including education, galleries and museums (Use class D2)
  - ▣ Assembly and leisure uses (Use class D2)
- The scale and massing of buildings within this area will consist largely of buildings between 5 and 8 storeys in height depending on their location and setting. Building heights can be increased towards the river away from the listed Wearmouth Bridge where increases in building heights can be accommodated within falling site levels.



Bonnersfield site, part of the residential quarter

- Additional floors may be permissible at key gateways and nodes where it can be demonstrated the increase in height will not have a detrimental impact on the setting of the cWHS or any other listed structure including the Wearmouth Bridges. However, buildings in such locations will not be permitted to exceed a height of 12 storeys (40 metres above ground level).
- It should be a priority to ensure all ground floor uses are active and offer a positive relationship with the public realm. Upper storeys of buildings should enable and maximise views of the Wearmouth Bridge and across the river corridor to increase levels of natural surveillance. Features such as building terraces and balconies will therefore be encouraged.
- Building blocks should consist of a mixture of linear and perimeter blocks to ensure opportunities are maximised to provide natural surveillance and have a positive relationship to the surrounding streetscape. Blocks should also be serviced internally to minimise the need for service entrances and areas to be accessed from the front of the building and accessed directly off areas of public realm.
- The dominance of roads which currently exist within the area will be reduced. Bonner's Field should be extinguished and replaced with a new pedestrian route to maximise the development potential for the wider area. Streets such as Howick Road and Palmer's Hill Road should be remodelled to give greater priority to pedestrians to improve pedestrian links down to the river.
- The public realm in the residential quarter will consist primarily of hard formal, high quality landscaping. Both the river shore and the routes linking down to the river will comprise of high quality materials, lighting, street furniture and tree planting to provide safe, attractive and vibrant areas of public realm. Opportunities to deliver features squares and public spaces should be explored in the north west corner of the area adjacent to the St Peter's Metro Station, as well as where routes link in with the river shore to act as gateways and nodes to improve pedestrian legibility.

### Business quarter

- The business quarter is located in the north east section of the masterplan area and is defined by Dame Dorothy Street to the north, the cWHS to the west, the university and river to the south, and residential development outside the masterplan area to the east.
- The quarter is located on a more gentle slope at the most eastern extreme of the masterplan area where the river gorge starts to open out into its estuarine setting.

This is envisaged to accommodate the potential redevelopment of the North Sands Business Park for a use and development which better relates to the setting of the cWHS and the surrounding uses of the University and National Glass Centre.

- The setting of this quarter located away from the city centre and between the cWHS and existing residential development suggests the site needs to act as a buffer between the activity associated with the cWHS and the quiet nature of the adjacent residential uses. This makes the site ideal for a quieter research and business environment which provides a relaxed but cosmopolitan character.



Business quarter



- Activity within the area will therefore be focused primarily along the frontage facing on to the cWHS and on to any internal/external formal public spaces provided within building blocks. Activity will consequently be focused away from adjacent residential uses.
  - Uses considered appropriate and to be encouraged in this area include:
    - ▣ Business uses including general offices and research and development (Use class B1 only)
    - ▣ Non-residential institutions including education, galleries and museums (Use class D2)
    - ▣ Ancillary retail, food and drink uses (Use classes A1, A3-A4)
    - ▣ Multi-storey car park (Use class sui generis)
  - Uses considered permissible with justification in this area include:
    - ▣ Hotel uses (Use class C1)
    - ▣ Food and drink uses including cafes, bars and restaurants (Use classes A3 and A4)
  - The scale and massing of buildings within this area will consist largely of buildings between 4 and 5 storeys in height depending on their location and setting. No building height will be permitted to exceed a height of 5 storeys (18m above ground level) to protect of the setting of the cWHS
  - It should be a priority to ensure all ground floor uses are active and offer a positive relationship with the public realm. Upper storeys of buildings should enable and maximise views of the cWHS and across the river corridor to increase levels of natural surveillance. Features such as building terraces and balconies will therefore be encouraged.
  - Building blocks should consist primarily of perimeter blocks to ensure opportunities are maximised to provide natural surveillance and achieve a positive relationship to the surrounding streetscape. Blocks should also be serviced internally to minimise the need for service entrances and areas to the front of buildings and access points directly off areas of public realm.
  - The public realm in the business quarter will consist primarily of hard formal, high quality landscaping. Opportunities to deliver an internal feature square within the quarter should be explored to ensure high levels of activity can be focused away from residential areas so as not to compromise adjacent residential amenity
- ### South shore
- As the name suggests, the south shore character area consists of the entire portion of the masterplan area which is located to the south of the River Wear, extending from the Wearmouth Bridge in the west to Corporation Quay in the east.
  - The steep topography of the south shore and its dense urban grain results in a landscape which overlooks and has a major impact on the character of the less steep north shore.
  - The area currently includes a range of uses including student accommodation, commercial space and a range of food and drink establishments which give the area a distinct character and role. The masterplan and design code therefore aims to support this role in the future through the redevelopment of sites at Numbers Garth, Scotia Quay and High Street East
  - Uses considered appropriate and to be encouraged in this area include:
    - ▣ Business uses including general offices and research and development (Use class B1 only)
    - ▣ Student and residential accommodation (Use class C3)
    - ▣ Hotel uses (Use class C1)
    - ▣ Food and drink uses including cafes, bars and restaurants (Use classes A3 and A4)
    - ▣ Ancillary retail uses (Use class A1)
  - Uses considered permissible with justification in this area include:
    - ▣ Leisure and assembly (Use class D1)
    - ▣ Non-residential institutions including education, galleries and museums (Use class D2)
  - This character area is identified as part of the Old Sunderland Riverside Conservation Area and development within the area is guided under the conservation area's Character Appraisal and Management Strategy (CAMS) which will need to be read and followed in conjunction with this document when bringing forward development in this part of the masterplan area.
  - In line with the CAMS document, the scale and massing of buildings within this area will consist largely of buildings between 3 and 4 storeys in height depending on their location and setting.
  - Additional floors may be permissible at key gateways and nodes where it can be demonstrated the increase in height will not have a detrimental impact on the setting of the conservation area, the cWHS and/or any other listed structure. This is especially the case at Numbers Garth which has been identified as a key gateway site to the wider city centre. However, buildings in such locations will not be permitted to exceed a height of 6 storeys (21 metres above ground level).

### Dame Dorothy Street

- The Dame Dorothy Street character area covers all residential land located within the masterplan area north of Dame Dorothy Street and includes Dame Dorothy Primary School and surrounding residential land.
- The aim of this character area is to preserve and enhance the area's role as a successful residential neighbourhood and to ensure its appearance does not change in a way likely to have a detrimental impact on the setting of the cWHS.
- The masterplan does not anticipate any major redevelopment of any of the sites within this character area, with changes more likely to occur through individual improvements to properties, through private investment from owners
- The exception to this is development along Lower Dundas Street which is considered to be the only major development opportunity in the wider character area. In such circumstances, building heights should not exceed 3 storeys (11m above ground level), with any development needing to demonstrate a positive impact on the area's character and appearance. Any development on this site which demonstrates the potential to have a negative impact on the setting of the cWHS will be resisted.
- The dominance of roads within this area will be reduced where possible to give a greater priority to pedestrians to improve links and permeability with the cWHS.
- Any new or improved public realm within the area will consist primarily of formal high quality hard and soft landscaping.



Numbers Garth site, South shore



## 9.2 Land use and flexible space

The masterplan will accommodate a wide ranging mix of uses capable of delivering a high quality urban quarter which will reinforce the role of the cWHS as a major city destination. Delivery of the masterplan should not only be able to attract a large number of visitors and tourists to learn, live, work and play within the area, but must also be able to cater for the existing local community and wider city centre by providing amenities which are currently not available in the area. Only when this balance is achieved can a truly successful and commercially sustainable redevelopment be realised.

In line with the aspirations of the masterplan and to ensure the highest possible design qualities are delivered, all developments within the masterplan area will also need to accord with the policies and guidance included within the Central Area Urban Design Strategy (CAUDS) SPD in addition to the policies and guidance included within this document.



Learning quarter

### Commercial and business uses

Within the masterplan area, a majority of the new commercial and business uses is likely to be focused on the large development sites around the cWHS extending down to the river, primarily within the learning and business quarters of the masterplan. Such uses, although likely to be on a much smaller scale may also be appropriate on the redevelopment sites within the south shore character area, or in existing vacant buildings where previously occupied buildings have become vacant and an alternative use is being sought.

Commercial and business use classes considered appropriate for the masterplan area include:

- A1 Shops (Ancillary)
- A3 Restaurants and Bars
- A4 Drinking Establishments
- B1 General Business
- D1 Non-residential Institutions
- D2 Assembly and Leisure

Whilst the above use classes have been identified as appropriate for the masterplan area, prospective developers and investors should bear in mind the likely impacts distance from the city centre is likely to have on development prospects. Areas such as the south shore and residential quarter are located within close proximity to the city centre and good transport links. Consequently, there may be scope to demonstrate that these areas maybe categorised as 'edge of centre' sites

which may be appropriate for city centre uses. In areas such as the learning and business quarters, their increased distance from the city centre and transport links may mean they are categorised as 'out of centre' locations and may therefore be less suitable for city centre uses.

As a consequence, while the city council will be supportive of high quality mixed use schemes across the masterplan area, proposals for such commercial uses may need to be supported by a sequential assessment which demonstrates the following:

- How the use is appropriate for its location and satisfies the aspiration of the masterplan
- How the use is complementary to surrounding communities
- How the use does not jeopardise the delivery of other major regeneration projects proposing the same use
- How the use does not jeopardise existing local facilities and infrastructure
- How the use is accessible to all

For further guidance on determining if a sequential assessment is needed and what should be included within a sequential assessment, developers should refer to the DCLG document 'Planning for Town Centres: A good practice guide on needs, impact and the sequential approach'.

## Residential uses

These uses will be primarily focused towards the west of the masterplan within the residential quarter. Such uses may also be appropriate within the south shore and Dame Dorothy Street character areas, although it is anticipated on a much smaller scale.

In other character areas, residential uses may be appropriate especially on the upper floors of buildings which may not lend themselves to commercial uses. In such situations, developers will have to demonstrate that any residential uses are an essential ancillary component of a wider mixed use development and that proposals do not conflict with the aspirations and viability of other character areas.

The residential use classes considered most appropriate for the masterplan area, subject to supporting relevant greenspace policy requirements include:

- C1 Hotels
- C3 Residential Dwellings

In line with the aspirations of the masterplan and to ensure the highest possible design qualities are to be delivered, all residential uses will need to accord to both the Building for Life Criteria and policy included within the Sunderland Residential Design Guide SPD. Proposals for residential uses on the site have to demonstrate the following:

- Does the accommodation mix reflect the needs and aspirations of the local community?
- Does the tenure mix reflect the needs of the local community?

## Other uses

In order to deliver a scheme which is truly sustainable and in order to realise the masterplan area's full potential, there are a number of uses which are not categorised above which may be appropriate on the site.

However, where such uses are proposed, proposals should be accompanied by a robust justification for that use and how it accords with the principles of the masterplan and design code. Any uses which are deemed to be in conflict with the masterplan and design code, and which are likely to have a detrimental impact on the area and its surroundings will be resisted. Consequently, if a proposed use conflicts with the advice contained within this document, the onus will be on the applicant to demonstrate and prove the following as part of any planning application:

- That the use proposed within the masterplan is no longer viable and can no longer be implemented
- That the use proposed is acceptable for the site and does not compromise the deliverability of any other part of the masterplan area

- That the proposed development does not compromise the deliverability of any other major development sites in the central area which were previously envisaged for the use proposed
- That the proposed development still accords with the aspirations and objectives of the masterplan and design code

Uses which will not be permitted within the masterplan area include:

- B2 General Industry
- B8 Storage and Distribution

## Flexible space

To deliver viable and sustainable development capable of meeting ever-changing market conditions, creating a scheme which is flexible and able to adapt over time is crucial to the longevity of the masterplan area. This is particularly relevant but not exclusive to commercial and business buildings which are likely to be occupied by a number of varying uses and tenants over the course of their lifetime. As a consequence, designing flexibility into development proposals should be a component part of the design rationale for any scheme.

To minimise the work required to change the use of buildings, flexibility can be achieved at the outset of a proposal by incorporating a number of key design features.

In buildings to be used for commercial and business uses, these include:

- Increased ground floor heights to a minimum floor to ceiling height of 2.8m will be required
- The inclusion of 'flexible frontages' to ensure shop fronts can be removed or changed without detriment to the upper floors of the buildings
- Flexible and open plan design to ensure internal walls can be built/ removed to allow ground floors to be subdivided and/or amalgamated depending on an occupier's needs
- Ensuring the design of floor joists and load bearing structures allows for future conversions
- Configuring internal circulation to allow potential future access to the upper floors independently of ground floor retail entrances

These principles apply equally to refurbishment projects and are not exclusive to new build schemes. Consequently, these principles may well be relevant to schemes on the south shore in the event of building plots being made available for redevelopment and/or refurbishment

### 9.3 Views and Vistas

At the heart of the masterplan and design code is the need to preserve and enhance the setting of the area's numerous heritage assets, namely that of St Peter's Church. Key to this is the protection and enhancement of views of the building and/or asset in question, and the need to harmoniously marry them into their surrounding context through the use of physical, visual and inferred linkages with the surrounding area.

As discussed in section 4.6 of this document, a number of key views have been identified which need to be addressed and enhanced through any new development being brought forward within the masterplan area. Consequently, the following design principles will need to be considered and addressed in any development likely to impact upon views of the following heritage assets:



View of St Peter's Church from the south shore



## St Peter's Church

- The identified viewshed and primary views of St Peter's Church as shown on the "Key views plan" on page 65 has an influence on development within all character areas of the masterplan, especially within the learning quarter, South shore and Business quarter character areas.
- When considering views taken from the river (primary views P1-P4), developers should be reminded that the opposite view is of equal importance and heritage value. Views of the church from the river should therefore be treated with equal importance as the corresponding view of the river from the church.
- Development within the identified viewshed will only be permitted when it can be demonstrated that development re-enforces and enhances the identified primary views.
- Whilst it is acknowledged and accepted that development within the viewshed may result in the partial loss of any given primary view, developers will need to demonstrate that any loss of views have been kept to an absolute minimum and that proposal have no detrimental impact on the view corridors identified on page 65.

- Where views remain, the onus will be on the developer to demonstrate that development enhances and protects the remaining primary view ensuring the view is not further eroded in the future.
- Developments which result in the significant loss of any primary view as identified on the views and vistas plan overleaf will not be permitted.

In addition to the above, the 'Area of greatest sensitivity' as identified on the views and vistas plan overleaf has been recognised due to the potential impact this area may have on the wider views of the church. Consequently, the council has worked in partnership with the land owner – Sunderland University – to identify the following additional design criteria which need to be adopted within development on this site.

- The topography of this area needs to be remodelled to ensure views are maximised between the north shore of the river and the church building. This should include the removal of retaining walls and structures where feasible with the aim of expanding existing views or opening up new views.

- A minimum of 2 new view corridors need to be accommodated within this area providing direct visual and pedestrian links between the church and the river. These corridors should be at least 20m in width and provide clear uninhibited views of the church when viewed from the north shore. The opportunity to provide additional corridors should also be encouraged.
- Buildings within this area need to be aligned accordingly to complement and reinforce views between the church and the river.



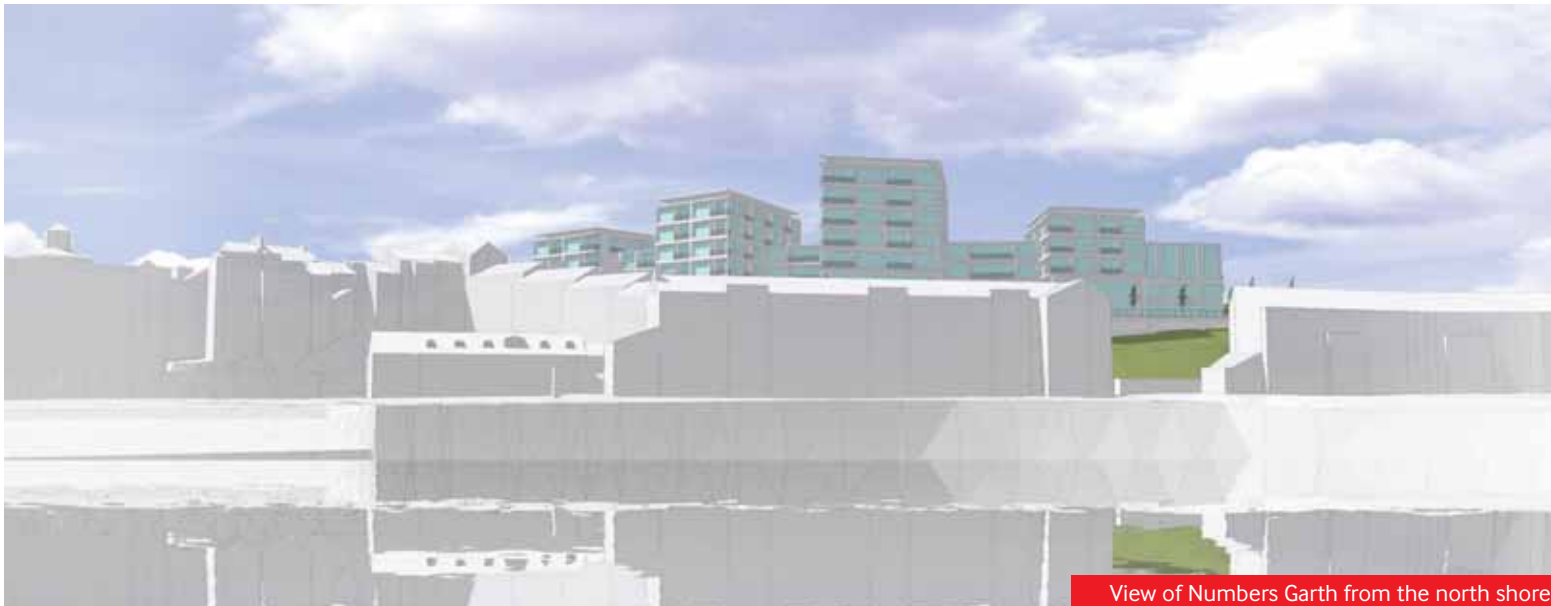
View of St Peter's Church through learning quarter

### River Wear Bridges

- The grade II listed Wearmouth Bridge and the Wear Rail Bridge have established themselves as the dominant feature within the lower reaches of the Wear Valley, and this should remain the case throughout the future redevelopment of the masterplan area.
- Both bridges are clearly visible from the north and south shore of the river from a majority of the masterplan area, and offer the area a clear and unique identity. Building heights and setting should seek to maintain and enhance these views as a matter of priority.
- When crossing the bridges, particularly the Wearmouth Bridge, pedestrians are offered panoramic views across the entire masterplan area, the wider river gorge, and the coastline beyond. These views should also be protected as a matter of priority.
- Whilst building heights are likely to vary across the masterplan area, and tall buildings may be considered appropriate in a number of locations, any proposals of a height, massing or layout which are likely to compromise views of and/or from the bridges will not be permitted.

### Old Sunderland Riverside Conservation Area

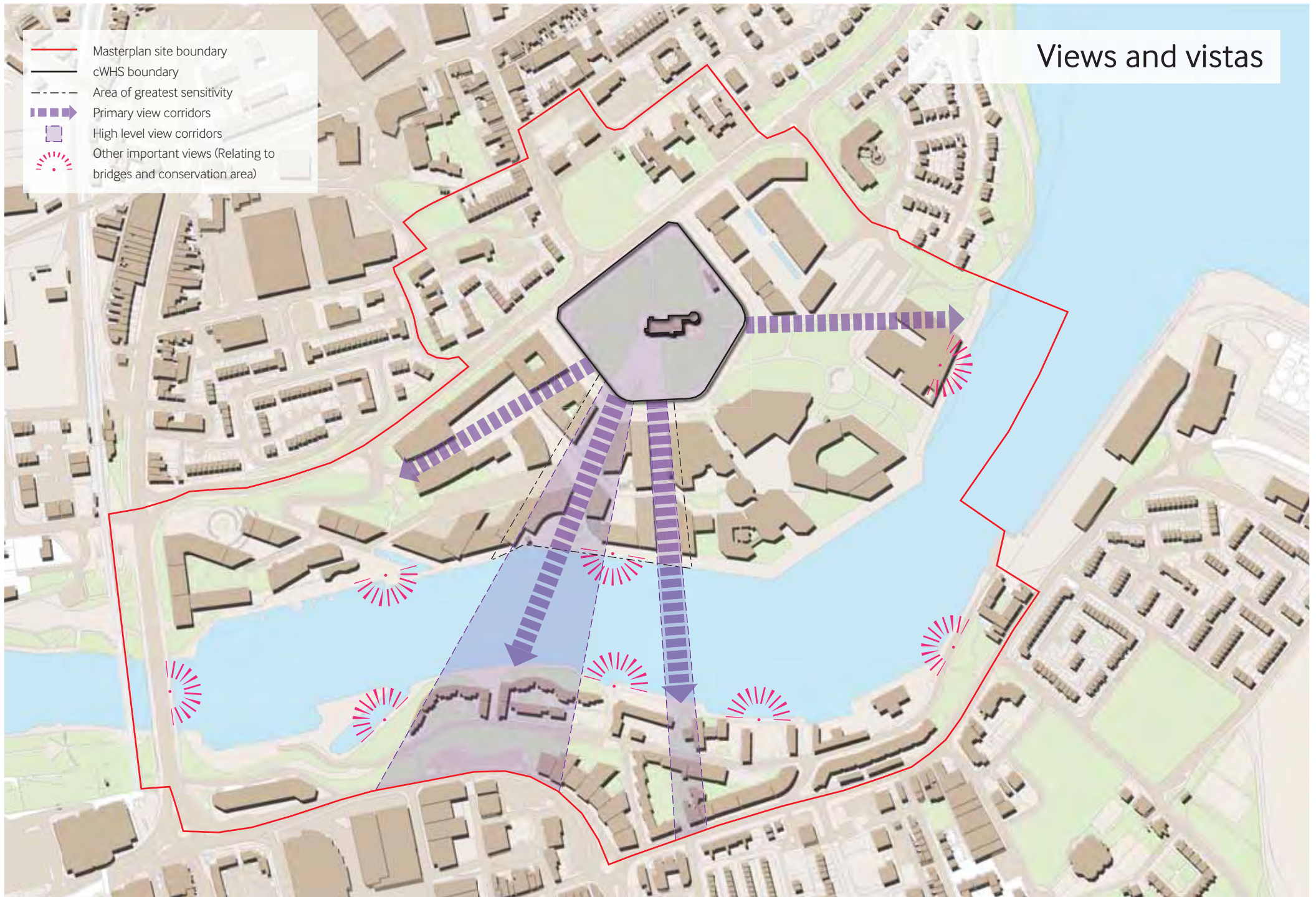
- Whilst one of the primary objectives of the masterplan is the enhancement of the setting of the cWHS including views into and out of the site, similar sentiments hold true for the setting of the Old Sunderland Riverside Conservation Area which covers the entire south shore character area of the masterplan and consists of a number of listed buildings, historic wharfs and historic streetscapes.
- The importance of the Old Sunderland Riverside Conservation Area is highlighted within the forthcoming Character Appraisal and Management Strategy (CAMS) being developed by the council to help manage future development within the conservation area, offering guidance with regard to building design and appearance.
- Any proposals for sites within the south shore character area and the Old Sunderland Riverside Conservation Area should pay due regard to the guidance included in both this and the CAMS documents, both of which are designed to be complementary to one another.
- Proposals which do not accord with the guidance offered in either document will not be permitted. For further information with regard to the Old Sunderland Riverside Conservation Area CAMS, please contact the council's Conservation Team on 0191 561 1515 or visit their website [www.sunderland.gov.uk/characterappraisals](http://www.sunderland.gov.uk/characterappraisals)



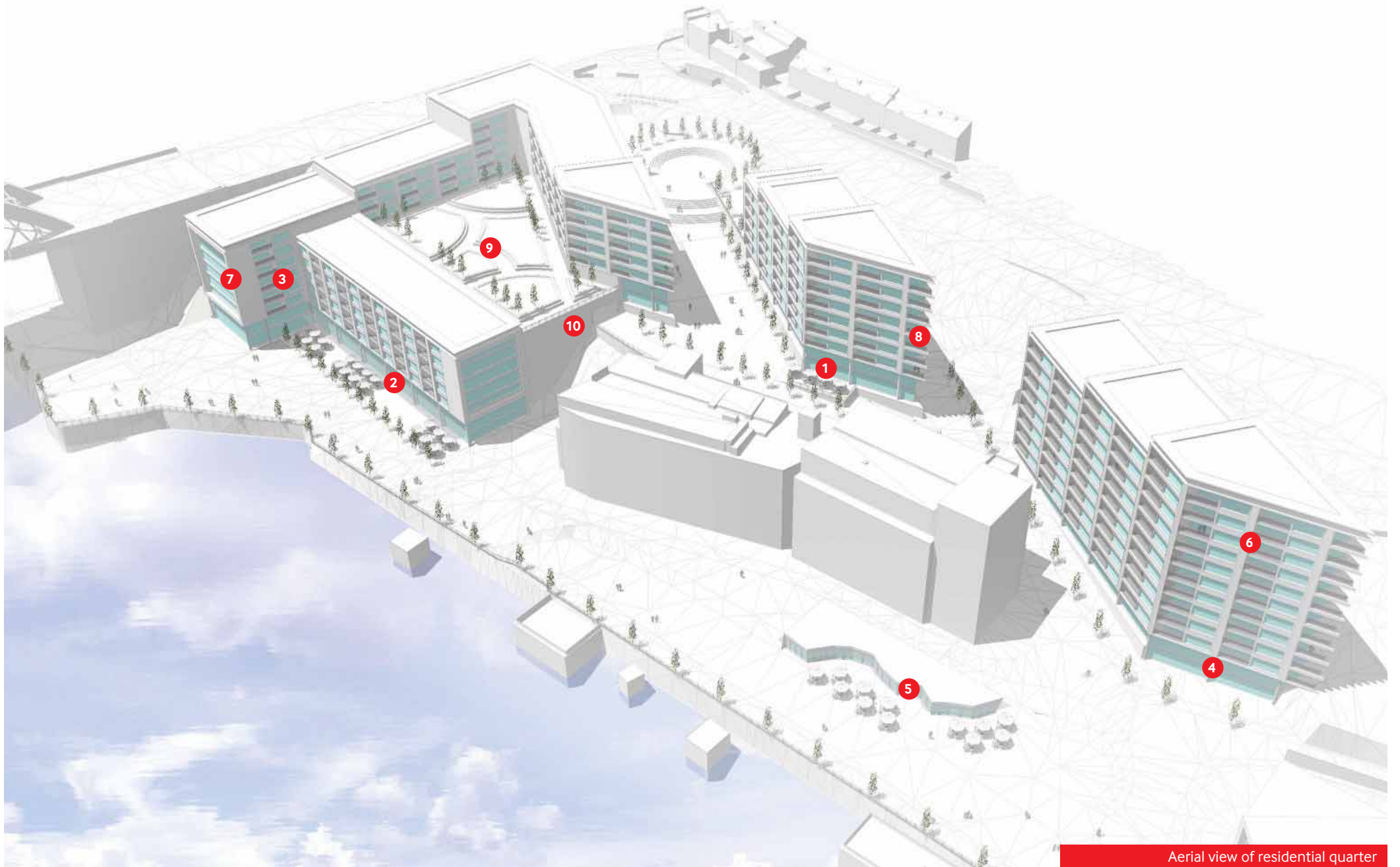
View of Numbers Garth from the north shore



## Views and vistas







Aerial view of residential quarter

## 9.4 Block principles

The Wearmouth Masterplan is based upon an urban structure made up of a range of perimeter and linear building blocks.

Perimeter blocks are an efficient and logical built form which help create a clear distinction between public frontages and private rears. Public and private space is clearly defined, natural surveillance over the public realm is encouraged, and route legibility is easy to interpret.

However, when maximising site efficiency, perimeter blocks are not always possible or appropriate, especially on narrow pieces of land or sites on steep topography. In such circumstances, linear blocks may be considered more appropriate.

To help determine which block type is appropriate for any given part of the site, the following key block principles will need to be addressed:

- The need to accommodate all the spatial needs of the development within the block and/or building line, including private car parking provision, any refuse and servicing provision, and any amenity space which may be required.
- The need to balance land efficiency with the need to deliver appropriate spacing standards to provide suitable levels of privacy and suitable levels of public open space.
- The need to balance the interests of providing safe and secure public space benefitting from natural surveillance and supervision whilst respecting residential rights to privacy and amenity.

Once it has been determined which block type best accommodates the above principles for the given part of the site, a number of key design criteria then need to be 'designed into' the blocks to ensure they meet the high design aspirations and objectives of the masterplan. Whilst the ways in which these criteria may be demonstrated will vary according to the use and scale of any given block, all the following criteria must be addressed as part of any proposed scheme.

### 1. Continuous street frontage

Frontages should provide outlook onto all areas of open public space. Prominent routes will benefit from the higher levels of active frontage, with less activity required on less prominent frontages. The majority of elevations fronting onto areas of public realm must be active. Areas of significant blank/inactive frontages will not be permitted.

### 2. Continuous building line

All new buildings shall conform to and enforce coherent building lines. Small variations such as setbacks and extrusions will be welcomed to create aesthetic interest. However the majority of buildings will need to conform to the same building line. Any proposals which are detrimental to and/or detract from the clarity of a building line will not be permitted.

### 3. Architectural variation between buildings

Any large/continuous frontage should be suitably broken up so as to better relate to the urban grain of the surrounding area and not dominate the streetscape. Whilst a consistent approach to quality and design standards will be adopted throughout, variations in the façade treatment and style such as a mix of gable/front elevations will be welcomed. Large/continuous façades which dominate their settings will not be permitted.

### 4. Flexible ground floor uses

All ground floor uses should be designed to be highly flexible to allow for alterations/conversions to be made with minimal disruption to adjacent properties. All ground floor properties must be designed to meet the specification discussed in section 9.2. Proposals which do not accord with this specification will not be permitted.

### 5. External activity to be encouraged

To encourage activity into areas of public realm and create a vibrant pedestrian experience, opportunities to encourage external activity at both ground floor and upper floors will be encouraged. Features such as outdoor seating areas, balconies and roof terraces will be welcomed.



Learning quarter frontage onto St Peter's Square

6. **Building heights should increase toward the river and with falling topography**  
To increase land efficiencies and to harness potential views across the river and wider area, building heights should be increased towards the river where increased building heights can be accommodated within lower land levels therefore lessening their visual impact on their surrounding setting.
7. **Building heights should decrease towards the cWHS, listed buildings and heritage assets, and with rising topography**  
To better relate the scale of proposed building with the site's heritage assets and surrounding urban form, building heights should decrease with increased proximity to listed and protected structures and with higher land levels to preserve the setting of such structures and the amenity of surrounding existing land uses.
8. **Feature buildings on prominent corners**  
Buildings should emphasise and celebrate prominent corners to improve legibility and to punctuate rooflines. The increase of building massing/ height, use of materials, increased levels of glazing/fenestration can suitably achieve this effect. Proposals which are considered not to make the most effective use of their setting will be discouraged.
9. **All private parking, servicing and refuse collection shall be accommodated within central courtyards or undercroft/ underground areas away from public view**  
Where perimeter blocks have been used, central courtyards should accommodate all required onsite parking provision, servicing and refuse space. Space within such courtyards can be optimised through the use of elevated garden/amenity space accommodated above undercroft service area. In linear blocks, undercroft or underground services areas are more appropriate due to the decreasing footprint of the block. Proposals which include front servicing of properties or which have services areas in clear view from areas of public realm will not be permitted.
10. **Single entrances to private parking and service areas**  
The number of entrances to central courtyards, undercroft and/or underground parking/service areas should be kept to a minimum in order to ensure street frontages are fully utilised to provide active frontages for natural surveillance onto areas of public realm. Consequently vehicular entrances should be located on the least prominent frontage of any building possible.



## 9.5 Building heights and densities

Delivering an appropriate variety of building heights and densities will help create a rich urban form which relates to the masterplan area's unique setting and landscape, and will represent a form of development which is attractive, dynamic and visually interesting to achieve the objectives of this document.

In general, building heights throughout the masterplan area will vary in height from between 4 to 8 storeys, although local variations on building heights have been determined in line with the character area principles discussed in section 9.1 of the code. Consequently, the massing of buildings through the masterplan area will vary from one part of the masterplan area to another helping to establish a legible urban form which is sensitive to the setting of numerous heritage assets including the cWHS and which harnesses the area's topographic characteristics.

The building heights plan overleaf gives an indication of the spatial variation in building heights across the masterplan area according to the identified character areas, and has been developed according to the following principles:

### St Peter's square

- As stated previously in this document, it is not envisaged that any new buildings will be erected in the St Peter's square character area

### Learning quarter

- New buildings within the learning quarter character area are envisaged to range in height from between 2 to 6 storeys or 7m – 21m Above Ground Level\* (AGL).
- Buildings fronting on to the cWHS will not exceed a height of 5 storeys or 17.5m AGL so to protect the setting of St Peter's Church.
- Increased building heights should be located towards the river where they can be accommodated within falling site levels
- Building heights should be decreased along Dame Dorothy Street where buildings pose a detrimental risk to the residential amenity of existing dwellings to the north
- Any building located within the identified viewsheds as discussed in section 9.3 of this document should be of a height and scale which complements and strengthens view corridors.
- Buildings of a height and massing which block viewsheds and view corridors to any significant and/or detrimental effect will not be permitted.

### Residential quarter

- New buildings within the residential quarter character area are envisaged to range in height from between 6 to 12 storeys or 21m – 42m AGL
- Buildings fronting directly on to the river shore located between the existing Bonners Raff development and the listed Wearmouth Bridge should not exceed a height of 8 storeys or 28m AGL so to respect the setting of the bridge and to continue a strong urban form already established by the Bonners Raff building
- Increased building heights should be located towards the river shore away from heritage assets such as the Wearmouth Bridge and the cWHS where they can be accommodated within the falling site levels
- Building heights should be decreased to the north, west and east of the character area where they pose a detrimental impact to the setting of heritage assets and risk damaging the residential amenity of existing dwellings along Dame Dorothy Street

### Business quarter

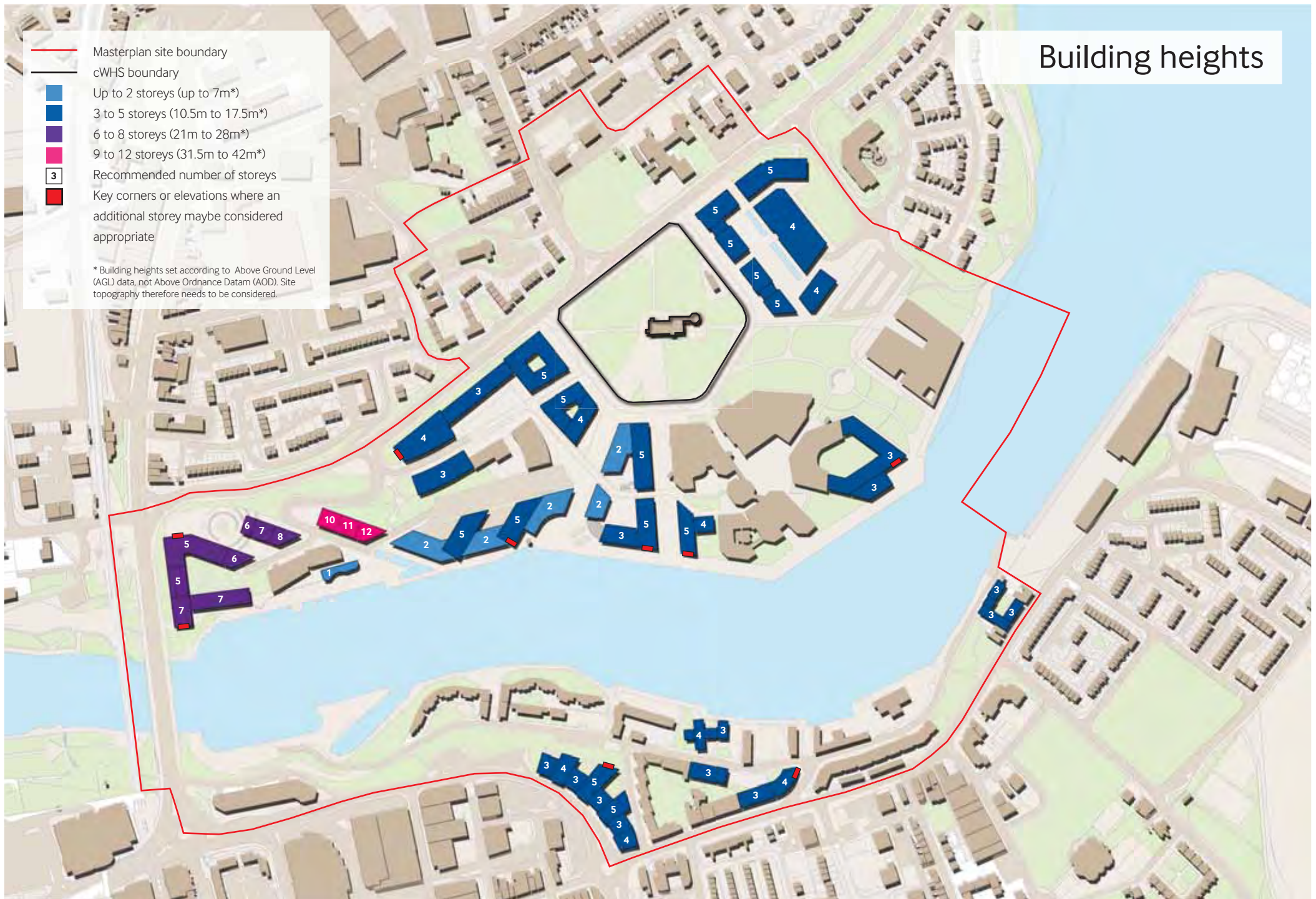
- New buildings within the business quarter character area are envisaged to range in height from between 3 – 5 storeys or 10m – 17.5m AGL
- Buildings fronting on to the cWHS will not exceed a height of 5 storeys or 17.5m AGL so to protect the setting of St Peter's Church.
- Building heights should be decreased towards the east of the site along Liberty Way where buildings pose a detrimental risk to the residential amenity of existing dwellings to the east

\* Above Ground Level (AGL) is measured from the lowest level at which any given building meets the ground. A standard floor to floor height of 3.5m has been used to calculate corresponding height limits.

## Building heights

- Masterplan site boundary
- cWHS boundary
- Up to 2 storeys (up to 7m\*)
- 3 to 5 storeys (10.5m to 17.5m\*)
- 6 to 8 storeys (21m to 28m\*)
- 9 to 12 storeys (31.5m to 42m\*)
- 3 Recommended number of storeys
- Key corners or elevations where an additional storey maybe considered appropriate

\* Building heights set according to Above Ground Level (AGL) data, not Above Ordnance Datum (AOD). Site topography therefore needs to be considered.





### South shore

- New buildings within the south shore character area are envisaged to range in height from between 3 – 6 storeys or 10m – 21m AGL
- New buildings which sit adjacent to any listed structure within this area will need to be of a suitable scale which complements and is not detrimental to the setting, appearance or character of the listed structure. Generally, the consequence of this is an expectation that any new buildings should not exceed the height of any adjacent listed structure.
- Increased building heights should be located in prominent locations away from any listed structures where they can help define and reinforce key gateway locations such as Numbers Garth
- In general, building heights should fall with the gradient of the topography in line with the areas distinctive character which has established over time.

### Dame Dorothy Street

- New buildings within the Dame Dorothy street character area are envisaged to range in height from between 2 – 4 storeys or 7m – 14m AGL
- Taking into consideration the residential character of the area, building heights within the character area should be influenced primarily by the need to protect the residential amenity of residents within the area

Building heights should be determined to complement the masterplan area's context and setting, emphasise and celebrate the area's key physical attributes, and support the delivery of a legible urban form. Building parameters as stated above should be adhered to as a matter of priority. Buildings which sit outside the identified scale parameters will be resisted by the council unless in extreme circumstances where it can be demonstrated that the building design is of an exemplar quality and is in the public interest of the masterplan and wider area.

Any building heights which do not accord with the principles of the design code will not be permitted.



Views of residential quarter from south shore



## 9.6 Gateways and landmarks

Landmarks and gateways help to provide reference points to emphasise a hierarchy of streets and spaces for all users, and play a fundamental role in contributing to the wider legibility of a development. When designed to a high standard, they can help add interest and variation to the skyline and can stand out from the context of their general surroundings.

In the case of the Wearmouth masterplan, they also play a key role in linking the three crucial components of the area: the city centre, the River Wear, and most importantly, the cWHS. A sequence of gateway spaces and landmark buildings can help provide fluid and legible connections which not only help to marry the cWHS into the adjacent city centre and river corridor, but also provide a distinctive, pleasant and memorable arrival experience to visitors, and help create a positive first impression to those who have never previously visited Wearmouth or the cWHS.

Five primary gateways have been identified as part of the Wearmouth Masterplan and Design Code:

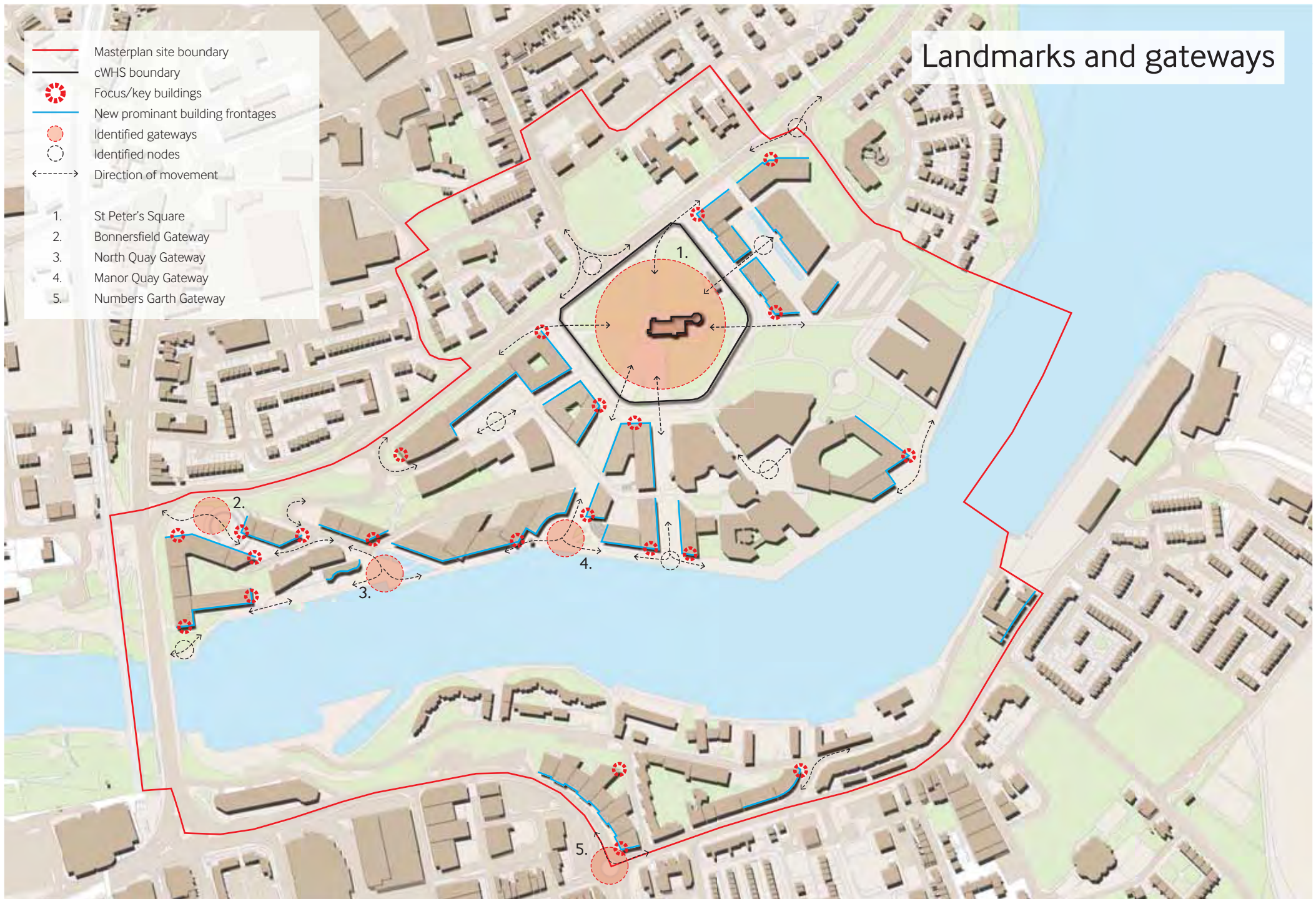
1. St Peter's Square
2. Bonnersfield Gateway
3. North Quay Gateway
4. Manor Quay Gateway
5. Numbers Garth Gateway

Each gateway has different roles to play within the masterplan according to their location, setting, and who is most likely to use them. The following principles have been developed to identify how their roles may be achieved.

### St Peter's Square

- At the heart of the masterplan, St Peter's Square is not only the single most important space within the masterplan area, but is also the largest gateway framing the historic St Peter's Church. The size, prominence and importance of this space suggests that it not only needs to act as a suitable setting for the cWHS, but also act as a gateway to those using the space to access the river corridor, and beyond to the city centre and sea front. The space therefore needs to be distinctive and vibrant in its appearance and form whilst being of a quality which is subtle enough not to overpower and compete against the unimposing St Peter's Church which must remain the most prominent feature in the space.
- Located in the middle of the masterplan area, the space is likely to be bound by a range of uses. Education and uses associated with the university to the south and west, potential business and commercial uses to the east, and existing residential uses to the north provide a range of opportunities to ensure the space is activated and celebrated through both the daytime and evening hours. Ancillary leisure uses should be encouraged at ground floor levels fronting on to the space making the area a prime location for small scale cafés, bars and restaurants which take advantage of the views overlooking the historic church.
- Buildings of a regular scale and massing should surround the space to reinforce the formal character of the area and provide a consistent and regular backdrop to the cWHS which neither competes or conflicts with the subtle form and appearance of the church building itself. Building heights should therefore range between 4-5 storeys tall (maximum 17.5m in height).
- Architecturally, the buildings framing this gateway must be of the highest design qualities, but be subtle and discrete in their appearance. Building frontages should face directly on to the church to animate the space, and building frontages should contain high levels of glazing and include features such as balconies and terraces to take full advantage of views overlooking the church. Statement architecture which is likely to detract attention away from the church building will not be encouraged in this location.
- The landscape treatment of the space will be formal but primarily green/soft in nature to help reinforce the space as a pleasant environment in which to enjoy and relax whilst viewing the church building and wider river views. The prominence of St Peter's Way will be downgraded through the use of surface materials to create a pleasant street with greater pedestrian priority and to encourage greater pedestrian movement between the cWHS and river.

## Landmarks and gateways





View of Bonnersfield Gateway

### Bonnersfield Gateway

- Located in the north west corner of the masterplan, the Bonnersfield Gateway stands not only as a gateway into the masterplan area, river corridor and cWHS, but also as a strategic gateway site to the wider city centre. Consequently, due to the gateway's proximity to the surrounding road network, Metro station and city centre, the Bonnersfield Gateway is likely to be the most highly used gateway in the masterplan area, and therefore needs to be of a suitable scale, massing, form and appearance to offer the best possible sense of arrival and first impressions to both pedestrians and vehicular traffic.
- On the edge of the residential quarter character area, this space will likely be surrounded by high-density, mixed use residential led development and could form the entrance to a new city centre residential community. Ancillary leisure uses should be encouraged at ground floor levels fronting on to the space making the area a prime location for small scale shops, cafés, bars and restaurants taking full advantage of the area's proximity to St Peter's Metro station and adjacent shops which bound North Bridge Street
- Building scale and massing at the gateway need to balance the need to provide a striking and notable gateway with the need to preserve and enhance the setting of the adjacent grade II listed Wearmouth Bridge. Consequently, building heights should range in height between 5-6 storeys tall (Maximum 21m in height).
- The appearance of buildings at this gateway will need to be distinctive and contemporary in form and language. The site's proximity to the Wearmouth Bridge and other listed buildings will require a high quality bespoke design response which is capable of making a statement in isolation whilst also being able to complement the setting of surrounding historic assets. The use of high quality materials will be key in achieving this.
- A formal but attractive hard landscaping scheme should be adopted in this space consisting of formal tree/shrub planting, high quality materials and high quality feature lighting to accommodate the high levels of pedestrian footfall likely to be associated with the space whilst also creating an attractive and pleasant space. The space may also be a suitable location for a major public art installation which may help re-enforce the space as a key gateway and node with a unique identity.



### North Quay Gateway

- Located along the north shore of the River Wear, the North Quay Gateway is the point at which pedestrians entering the site via the Bonnersfield Gateway meet the river, and have the choice of heading west inland along the river, or east towards the cWHS and the coastline beyond. Offering panoramic views along the river, the south shore and the Wearmouth Bridge, this major junction represents a major opportunity to provide a unique, vibrant, gateway space at the heart of the residential quarter.
- Away from major roads and associated traffic noise and located at the centre of the residential quarter, this space has the potential to act as the primary hub to a new city centre residential community. Ancillary leisure uses will be strongly encouraged at ground floor levels fronting on to the space making the area a prime location for small-medium scale cafés, bars and restaurants taking full advantage of views across the river and helping deliver the council's strategic aspiration of delivering an exciting and vibrant quayside area.
- Building heights in this location will be allowed greater flexibility due to falling land levels and the increased distance from both the cWHS and Wearmouth Bridge. Consequently, building heights will be permitted to increase to a maximum height of 12 storeys (Maximum 42m), again helping to reinforce this space as a hub within the wider masterplan area.
- Buildings at this gateway will also be given greater architectural freedom in order to create a truly unique environment. Buildings should be architecturally distinctive and contemporary in form and language. Building frontages should front directly onto the gateway to help animate the space. Building fenestration should consist of high levels of glazing to take full advantage of the area's south-facing aspect and include features such as balconies and roof terraces to harness views across the river.
- A formal but attractive hard landscaping scheme should be adopted in this space consisting of formal tree/shrub planting, high quality materials and high quality feature lighting to accommodate the high levels of pedestrian footfall likely to be associated with the space whilst also creating an attractive and pleasant environment. The use of water within the space should also be explored through the installation of either streams or water features to help celebrate the area's relationship with the river and to assist in the creation of an attractive feature space.

### Manor Quay Gateway

- Acting as the terminus to the boulevard which links the cWHS with the river, the Manor Quay Gateway acts as the fundamental component which gels a large area of the masterplan area together and re-enforces the historic relationship between St Peter's Church and the River Wear. Located beneath the cWHS and bound by the residential quarter to the west and the learning quarter to the east, the Manor Quay Gateway is responsible for both celebrating the area's rich heritage as well as bringing together a range of varying uses to create a truly vibrant and diverse gateway.
- Similar to that of the North Quay Gateway located away from traffic noise and along the river shore, this gateway represents a significant opportunity to achieve the council's aspiration of delivering an exciting and vibrant quayside area. Active ground floor uses will therefore be strongly encouraged in this area making the space a prime location for small-medium scale cafés, bars and restaurants taking full advantage of the unobstructed vista of the cWHS and panoramic views of the river corridor and bridges.
- The gateway's location within the middle of the identified viewshed of the cWHS as discussed in section 9.3 of this document means that building heights around this gateway will be tightly controlled. Whilst building heights bounding the viewshed will be able to go significantly higher, the buildings immediately adjacent to the Manor Quay Gateway will be limited to 2-4 storeys in height (Maximum 14m).
- Architecturally, the buildings framing this gateway must be of the highest design qualities, but again be mindful of the need to be subtle and discrete in their appearance so not to detract attention away from the views of the church building. Building frontages should front directly onto the gateway to help animate the space. Building fenestration should consist of high levels of glazing to take full advantage of the area's south facing aspect and include features such as balconies and roof terraces to harness views across the river.
- A formal but attractive hard landscaping scheme should be adopted in this space consisting of formal tree/shrub planting, high quality materials and high quality feature lighting to accommodate the high levels of pedestrian footfall likely to be associated with the space whilst also creating an attractive and pleasant environment. The space will also be defined by two sets of grand stairways – one leading to the cWHS, the other leading into the university – which should be integrated as a component feature of the landscaping for this gateway.



View of Manor Quay Gateway and St Peter's Church

### Numbers Garth Gateway

- Similar to the Bonnersfield Gateway, the Numbers Garth Gateway stands not only as a gateway into the masterplan area, river corridor and Old Sunderland Riverside Conservation Area, but also as a strategic gateway site to the wider city centre. Due to the gateway's proximity to the surrounding road network and city centre, whilst likely to be the gateway least used by pedestrians entering the masterplan area, the Numbers Garth Gateway is a prominent and important gateway of strategic value.
- Its location within the south shore character area and the varied uses which currently surround the site suggests that development at this gateway could take a number of forms and uses, although it is envisaged to come forward as part of a mixed use, residential or commercial lead scheme. Whilst active ground floor uses are to be encouraged in this location, external activity is likely to be located away from the gateway as a consequence of high traffic levels and associated noise.
- Building heights in this location will be allowed greater flexibility due to increased distance from both the cWHS and Wearmouth Bridge, although will still need to be respectful of the surrounding Old Sunderland Riverside Conservation Area within which this gateway is located. Consequently, building heights will be permitted to increase to a maximum height of 6 storeys (Maximum 21m).
- The appearance of buildings at this gateway will need to be distinctive and contemporary in form and language due to its prominent location within the river valley. The site's proximity to a conservation area and a number of listed buildings will require a high quality bespoke design response which is capable of making a statement in isolation whilst also being able to complement the setting of surrounding historic assets.

### Nodes

- Nodes have been identified as important crossing points or junctions throughout the area which, whilst not of a scale or prominence similar to that of gateways, still require acknowledgement through the urban fabric and demarcation as important spaces to help improve the legibility of the wider area.
- Nodes are to be celebrated to a lesser extent than gateways to ensure a coherent hierarchy of streets and spaces is evident and not confusing to the user. Celebration of such nodes will therefore be focused through variations in the style of public realm, surface materials, street furniture and planting instead of being identified through the surrounding built form



## 9.7 Building types and frontages

Having defined how building blocks will be structured and identified the role gateways will play at different locations around the area, it is important to ensure that the streets and routes across the area also benefit from the same level of quality control standards and codes. Gateways and high quality statement buildings are all important components to a masterplan, but it is the routes, spaces and buildings which bound them which are responsible for merging a development together and creating a truly successful, consistent and comprehensive development.

How streets are animated and defined will inevitably depend heavily on the uses that bound them. A commercial street with commercial frontages will have a totally separate character and will be used in a different manner to similar streets in residential areas with residential frontages. As a consequence, to ensure streets, routes and space are defined and enclosed by appropriate frontages, the following design codes and principles have been developed:

1. A range of ground floor unit sizes need to be provided along each frontage to encourage a variety of uses and to ensure bias is not given to any particular use along any given frontage. The role of the masterplan is to seek the development of a truly sustainable and mixed use environment which should seek to support both small, medium and large scale businesses.
2. Building lines should be largely continuous with breaks in the building line only being permitted to provide service access into the building block. However, such instances should be kept to a minimum, and placed in an appropriate location so as not to detract from the quality and strength of the building line. Proposals which include a significant number of vehicle/service entrances along primary frontages will not be permitted.
3. A majority of all ground floor frontages bounding the public realm must be active, with activity being provided through the following building features:
  - ▣ Primary and secondary pedestrian entrances which provide a clear physical link between the external public realm and the interior of a property
  - ▣ Primary commercial windows which provide a clear visual link between the external public realm and interior of a property.

Ground floor frontages which bound areas of public realm which are not predominantly active will not be permitted. Building features which are NOT considered appropriate for providing activity along a building frontage include:

- ▣ Emergency exits which do not provide a clear physical or visual link between the external public realm and interior of a property
- ▣ Secondary commercial windows which do not provide a clear visual link between the external public realm and interior of a property
- ▣ Fake or blocked commercial windows which are used for internal display purposes only and do not provide a clear link between the external public realm and interior of a property
- ▣ Non-transparent frontages



View along the quayside, learning quarter

4. Frontages above ground floor level which front onto areas of public realm should be largely transparent to encourage natural surveillance of the public realm, harness views, and to take advantage of the area's south facing aspect. Features which promote external activity and natural surveillance above ground floor levels such as roof terraces and balconies will be strongly encouraged where such features pose no risk to the amenity and privacy of existing residents. Large areas of building frontage which front onto areas of public realm that are not suitably active/transparent will not be permitted.
  5. Dynamic building frontages should be included to articulate building façades through use of building heights, architectural detail, punctuated rooflines, material changes and use of colour.
  6. The design of corner building plots should be suitably integrated into the rest of the building frontage and form a component part of the building's wider design. Gable elevations should be activated and treated in the same way as all other commercial and leisure frontages. Corner units should make the most efficient use of their dual aspect and likely prominent position in the wider streetscape.
  7. Building frontages should be designed to activate areas of public realm through both daylight and evening hours, and as a consequence should incorporate appropriate provision for external lighting. This should be both in the form of wall mounted or free standing lighting to illuminate areas of public realm, but also architectural and/or feature lighting which can draw attention to key buildings and design features to help improve the area's night time legibility.
  8. Commercial signage should be designed to form a component part of the overall design of building frontage to improve the legibility of the street and to reduce clutter from within a façade.
- For further details and clarification with regard to the design of commercial, residential or retail frontages, the council has a range of documents that should be read in conjunction with this code. They include the Residential Design Guide SPD, the Sunderland Central Area Urban Design Strategy SPD and the forthcoming Shopfront Design Guide SPD.





View along secondary shared street, St Peter's Square

## 9.8 Street types

Whilst the quality of the built form is of major importance when assessing the success of a scheme, of equal importance is the layout, hierarchy and quality of the streets and routes running through the site. Buildings and structures across a site can help assist site legibility, but without an easy to interpret hierarchy of streets, built and designed to meet the needs of its users, the legibility of the built form can soon become confused.

Streets and roads should efficiently move users into, around and through a site without cost to the scheme's quality, function and appearance. Roads can no longer inhibit pedestrian movement, should adequately accommodate the needs of but not be biased towards vehicular traffic, and should positively contribute to the qualities of the public realm across a development. Therefore the primary objective behind setting design codes for streets is to provide a high quality and legible hierarchy of streets across the site which positively contributes to the wider design objectives of the masterplan.

To rationalise this approach, any new roads or existing roads which are to be remodelled as part of the masterplan have been categorised within the following street hierarchy:

- Primary roads
- Secondary or shared surface streets
- Pedestrian boulevards
- Quayside promenade

The following street dimensions have been produced using guidance contained within Manual for Streets (Department for Transport). Such dimensions should be adhered to as a matter of priority, although localised departures from these dimensions will be permitted in the interests of catering for suitable refuse collection, servicing and turning space provision.

Any proposals which demonstrate a significant departure from the dimensions shown will be resisted.



## Street types

- Masterplan site boundary
- cWHS boundary
- Primary roads
- Secondary or shared surface streets
- Pedestrian boulevard
- Quayside promenade
- Street tree planting



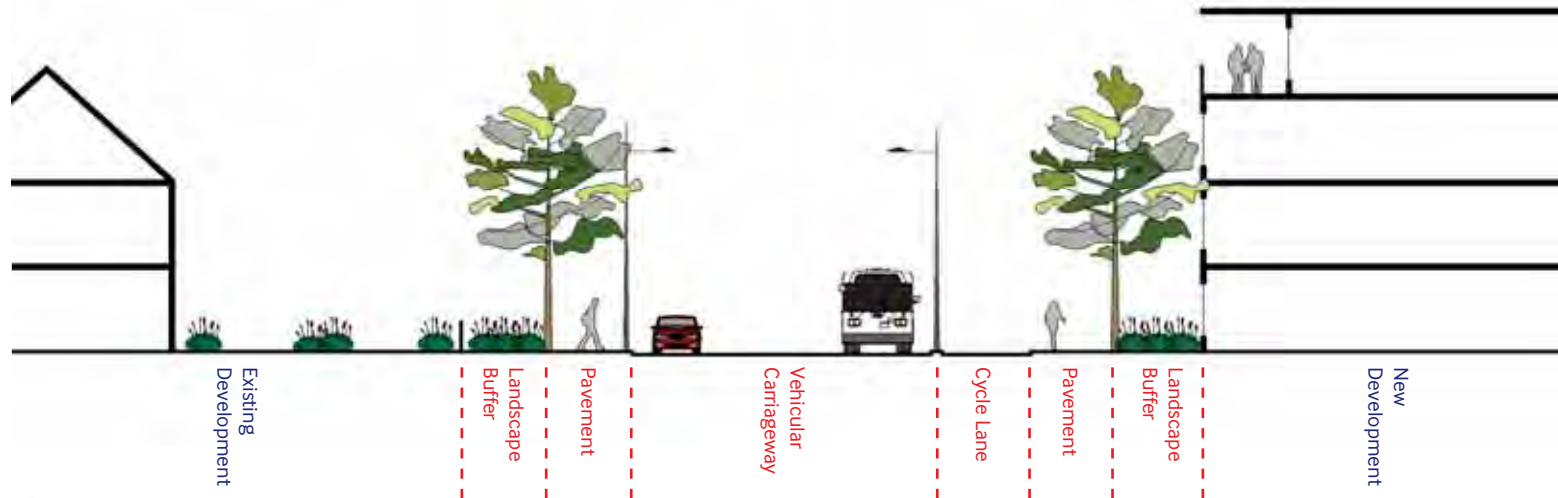
## Primary roads

The primary roads within the masterplan area include Dame Dorothy Street and North Bridge Street on the northern shore and West Wear Street on the south shore. These streets clearly define the masterplan area with the masterplan boundary following the alignment of one or more of these routes. Given their roles within the wider road network of the city, these routes will continue to fulfil their roles as the primary links connecting the coast and suburbs of Roker, Fulwell and Southwick with the city centre.

The alignment of these routes will not vary significantly from their current position, but changes in the road design will be undertaken to address issues which currently exist with regard to their dominance and the detrimental impact on pedestrian movement in the area.

The key design considerations relevant for such routes are as follows:

- Defining the edges of the masterplan area and bounding a range of existing land uses including those associated with city centre and residential areas, primary roads will need to be designed to accommodate the highest vehicular and pedestrian traffic flows in a harmonious way which suitably meets the needs of vehicular uses without detriment to the pedestrian environment.
- Defining the edge between new areas of development associated with the masterplan and existing uses within the areas around the masterplan, primary roads will have to be highly permeable to be able to accommodate and encourage the high pedestrian flows between the masterplan area and its surroundings.
- Spacing distances between buildings will also vary along these roads, however a distance of 20 metres between building frontages should be applied as a minimum so to protect residential amenity yet provide a sufficient level of enclosure to streets.
- Vehicular carriageways will retain their ability to accommodate between 3 and 4 lanes of traffic suitable for adoption as a bus route. However, where possible, efforts should be made to reduce the levels of on street parking along primary roads to relieve issues of congestion, reduce the visual dominance of the road, and maximise opportunities to increase the level of pavement provision and soft planting.
- Pedestrian footway provision will be maximised along such routes to counteract the dominance of the road, but also to accommodate the flow of pedestrians crossing and moving along the road. The width of the pedestrian footway either side of the road is likely to vary along the length of the road, but a minimum provision of 3m should be accommodated along both sides of these routes.
- A segregated 3m wide (minimum) cycle lane set back from the road by approximately 1m should also be included along at least one side of primary roads to support the council's aspiration of encouraging a modal shift towards more sustainable forms of transport.
- To mitigate against the impacts of noise associated with such routes on adjacent uses, a minimum 3m buffer strip should be accommodated along either side of primary roads between the pedestrian footway and the building line. This will also act as defensible space clearly defining the boundary between public and private space.
- Any street furniture including benches, bins, planters and lights should be suitably aligned along the pavement so to reduce the impact of street clutter and to increase pedestrian legibility along the road. This should also ensure the pedestrian capacity of the pavements is maximised.



Primary street cross section



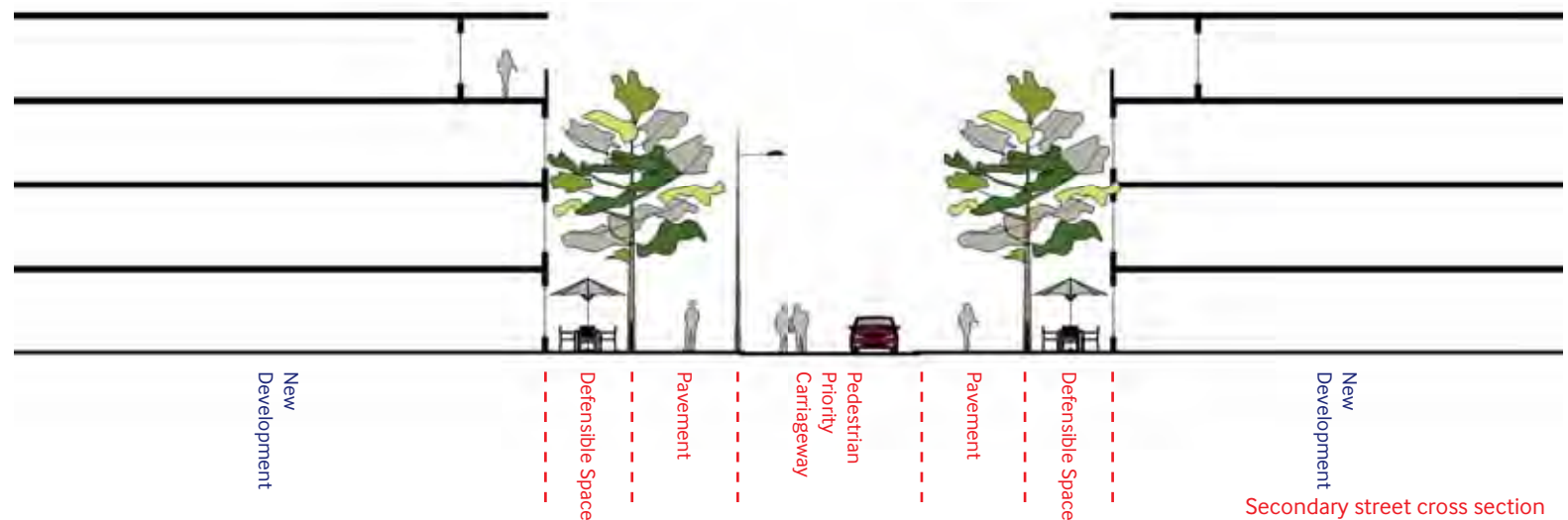
## Secondary shared surface streets

The role of secondary shared surface streets is to allow vehicular access to developments throughout the masterplan area whilst having the lowest possible impact on the quality of the surrounding public realm. Such streets should be designed to accommodate vehicular 'access only' and discourage through traffic so to give pedestrian flows priority over vehicular traffic. Consequently, such streets should have a distinct pedestrian character, the design of which is heavily biased towards providing a pleasant pedestrian environment.

The key design considerations relevant for such routes are as follows:

- Located throughout the masterplan area, secondary and shared surface streets need to be designed as a component part of the area's wider public realm to ensure such streets positively relate to the character of the area.
- Spacing distances between buildings will also vary along these roads, however a distance of 20 metres between building frontages should be applied as a minimum so to protect residential amenity yet provide a sufficient level of enclosure to streets.

- Vehicular carriageways will be designed to give pedestrians priority. They must be of a width capable of accommodating two lanes of passing traffic, although widths will be kept to a minimum, and road surfaces will reflect a shared surface style through the use of high quality materials and shallow curb lines. This will act as an informal traffic calming feature to ensure traffic speeds are kept to a minimum reinforcing the masterplan's street hierarchy. The width of the vehicular carriageway will therefore be restricted to a maximum width of 6m.
- Pedestrian footway provision will be maximised along such routes to counteract the impact of the vehicular carriageway, but also to accommodate the high flow of pedestrians likely to use the street. The width of the pedestrian footway either side of the road is likely to vary along the length of the road, but a minimum provision of 3m should be accommodated along both sides of these routes.
- The public realm and footway provision either side of the street will be increased at pedestrian junctions to accommodate nodal point and gateway spaces and the increased pedestrian movements associated with these areas.
- To accommodate and encourage outdoor/external activity associated with any leisure or entertainment uses accommodated within the masterplan, a minimum 3m strip of defensible space will be accommodated along each side of the street between the pedestrian footway and the building line to allow for outdoor activity. This should encourage businesses to use external spaces for seating and help increase levels of supervision and activity along the street.
- Any street furniture including benches, bins, planters and lights should be suitably aligned along the pavement so to reduce the impact of street clutter and to increase pedestrian legibility along the road. This should also ensure the pedestrian capacity of the pavements is maximised.



Secondary street cross section

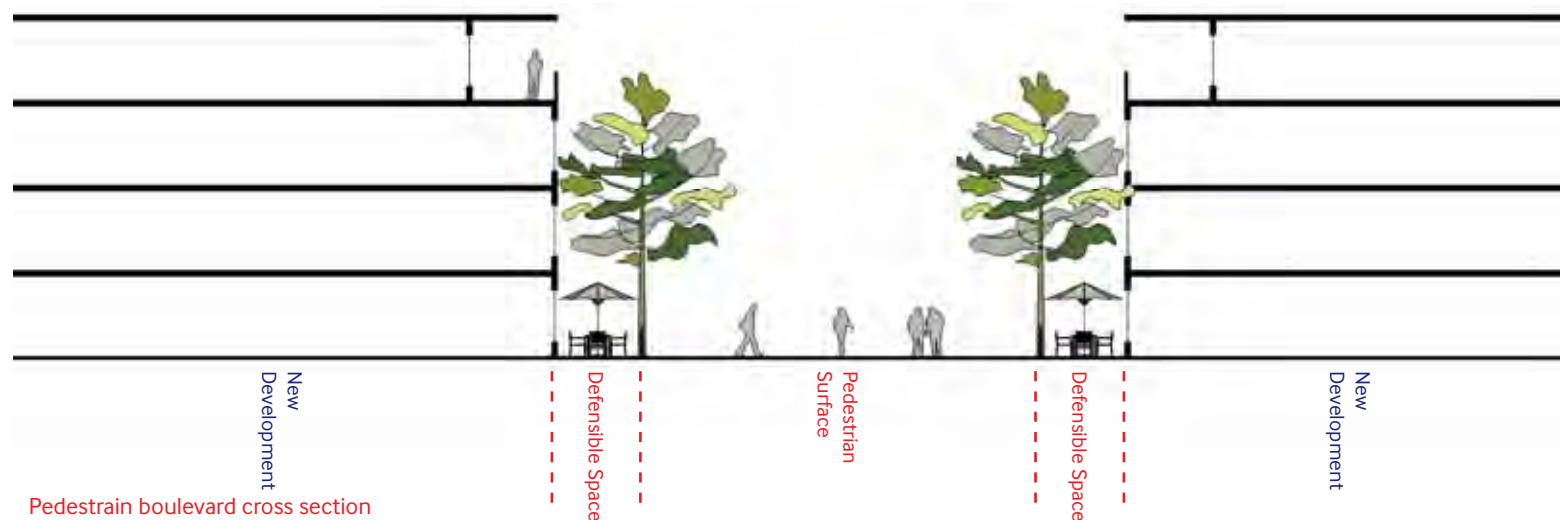


## Pedestrian boulevards

There are three pedestrian boulevard routes identified within the masterplan area, and whilst they are the least used category of street within the street hierarchy, they are considered to be one of the most important design features within the wider masterplan. Two pedestrian boulevards form the fundamental visual, physical and pedestrian connections which reinforce the historic link between the cWHS and the river. They will need to accommodate the important view corridors which are crucial to protecting the setting of St Peter's Church, as well as accommodating high levels of pedestrians likely to be moving between the vibrant river front and the historic cWHS.

The key design considerations for these routes are as follows:

- The pedestrian boulevards will form the primary pedestrian axes through the masterplan area and as a consequence will need to adequately accommodate the likely high pedestrian flows to ensure this route remains pleasant, uncongested and easy to use through all hours of the day, from early morning to late evening.
- Due to their location down the northern slopes of the river valley, the likely gradient of these routes may preclude their suitability to accommodate vehicular traffic along the length of the boulevards. Notwithstanding this, whilst these routes will be designed as pedestrian routes, suitable provision needs to be made to allow access to emergency vehicles at various points along each of the boulevards in case of emergency.
- To reinforce the role of the boulevards as the primary pedestrian routes through the site, footways will be the widest along this route. Consequently, boulevards will have to satisfy a minimum width of 20m (distance between building lines), within which a pedestrian footway of at least 14m in width will need to be accommodated. These widths will allow for suitable spacing standards between buildings to be accommodated, as well as allowing ample space for street furniture and planting without compromising the pedestrian capacity of the route.
- The provision of public realm and footways shall be increased at pedestrian junctions to accommodate nodal point and gateway spaces and the increased pedestrian movements associated with these areas
- To accommodate and encourage outdoor/ external activity associated with any leisure or entertainment uses accommodated within the masterplan, a minimum 3m strip of defensible space will be accommodated along each side of the street between the pedestrian footway and the building line to allow for outdoor activity. This should encourage businesses to use external spaces for seating and help increase levels of supervision and activity along the street.
- The provision of street furniture along this route will also have to accommodate pedestrian flows along the boulevard. Furniture should therefore allow suitable opportunities for people to stop, sit, relax and enjoy the surrounding environment without compromising the ability for other users to move freely along the route.



Pedestrian boulevard cross section

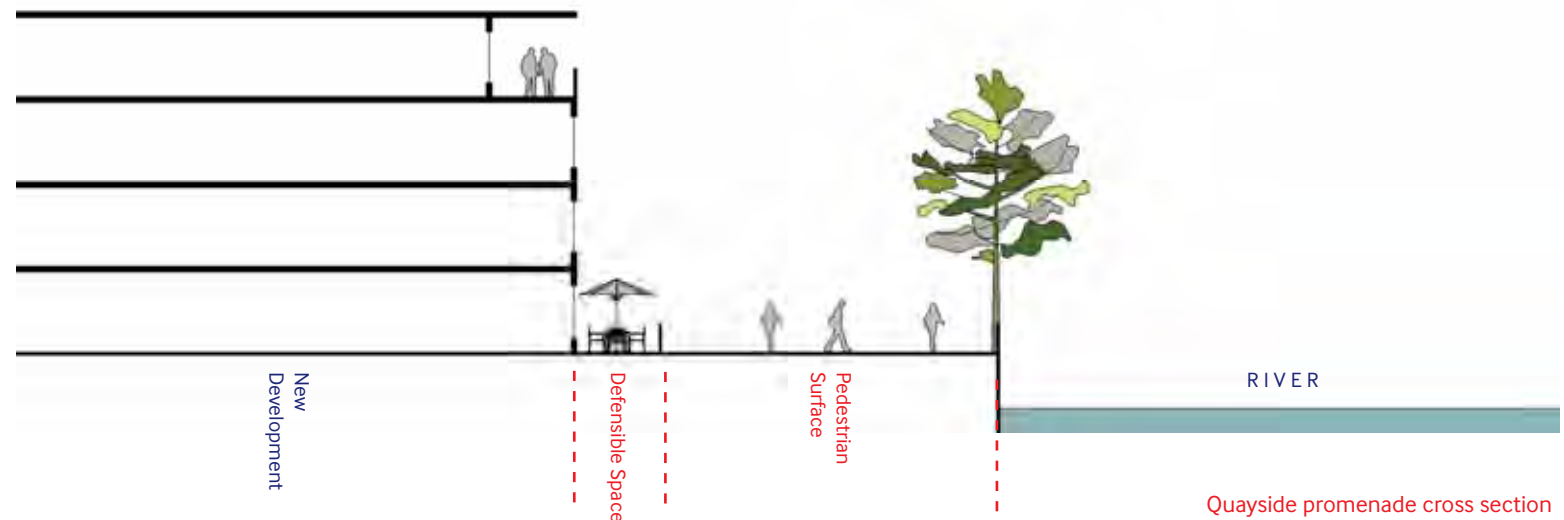
- Street planting will be incorporated along the boulevard to help add aesthetic value to the street and to improve visual amenity. Mature formal tree planting along either/ both sides of the route will help define the role of the boulevard in the wider route hierarchy of the masterplan, and also act as a suitable mechanism to reduce the impacts of wind, an issue which will need to be mitigated through the design of the street. Notwithstanding this, any street furniture or planting which significantly compromises views of the cWHS along the boulevards will be resisted.

## Quayside promenade

The quayside promenade is the route which runs along the length of the north shore of the river running from the Wearmouth Bridge in the west to the National Glass Centre in the east. The route will define the boundary between new development to the north and the open River Wear to the south. It will also form part of the existing pedestrian and cycle network which runs from the coast along the full length of the river. Consequently this very popular and highly used route will be pedestrian priority and will need to accommodate a wide range of leisure, retail and entertainment uses which will help create a vibrant and exciting quayside environment.

The key design considerations included within this route are as follows:

- The quayside promenade will form the primary east-west pedestrian axis through the masterplan area and as a consequence will need to accommodate the high pedestrian flows likely to ensure this route remains pleasant, uncongested and easy to use through all hours of the day, from early morning to late evening.
- Pedestrian footway provision will be maximised along this route to reinforce the role of the promenade as a key pedestrian route along the river as well as accommodating the high flow of pedestrians likely to use the route. Consequently, a significant provision of pedestrian priority surface will need to be accommodated along the length of the route, with a minimum width of 10m.
- Whilst the promenade will be designed to give pedestrians priority, suitable access provision will need to be accommodated to ensure access can be gained by emergency vehicles. Consequently, the pedestrian priority surface must be able to accommodate a route clear of street furniture and planting with a minimum width of 4m along the entire route of the promenade.
- The provision of public realm and footways shall be increased at pedestrian junctions to accommodate nodal point and gateway spaces and the increased pedestrian movements associated with these areas
- To accommodate and encourage outdoor/ external activity associated with any leisure or entertainment uses accommodated within the masterplan, a minimum 3m strip of defensible space will be accommodated along each side of the street between the pedestrian footway and the building line to allow for outdoor activity. This should encourage businesses to use external spaces for seating and help increase levels of supervision and activity along the street.
- The provision of street furniture along this route will also have to accommodate pedestrian flows along the boulevard. Furniture should therefore allow suitable opportunities for people to stop, sit, relax and enjoy the surrounding environment without compromising the ability for other users to move freely along the route.



Quayside promenade cross section

## 9.9 Access and servicing

The success of the masterplan is heavily dependent on the ability of the scheme to deliver adequate vehicular access provision in order to accommodate the traffic movement the masterplan creates. This includes providing adequate provision for visitors accessing the area, residents living in the area, businesses operating out of the area, as well as the suitable management of services supplying the area.

These issues should represent a key design consideration for any development of the area, and should be an important component driving the design of any scheme. Notwithstanding the above, providing suitable access to properties should not come at the expense of other design qualities, and so the need to consider access as a component as part of a wider comprehensive scheme is imperative.

To support this approach, the following design principles have been identified and should be taken into consideration when designing the access provision of any proposal.

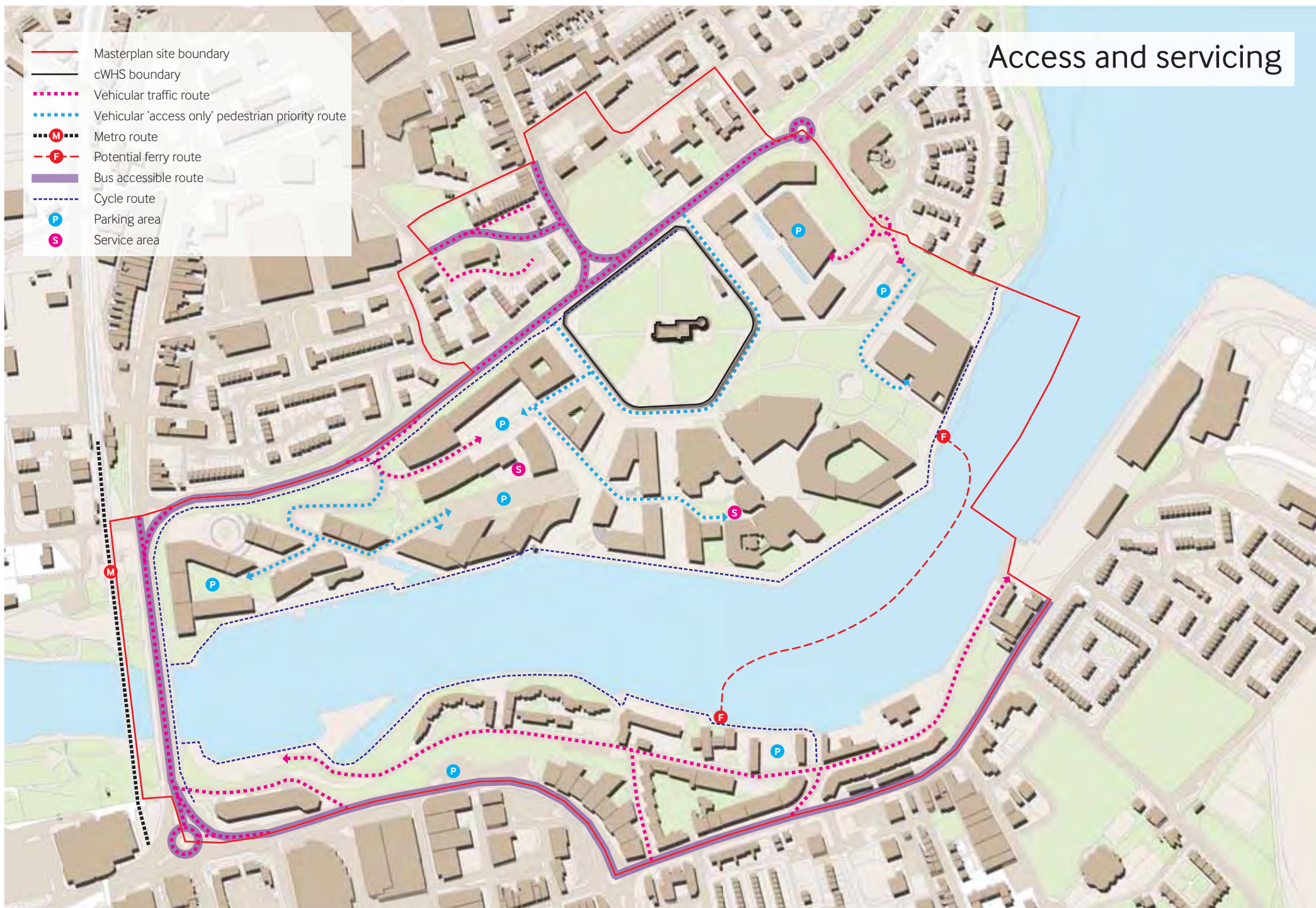
### Private commercial parking and servicing

- Parking provisions for commercial uses should be provided in line with the maximum provisions proposed in PPG13: Transport according to their use, scale and location. This is in line with policy T23A of the council's UDP Alteration No. 2 which seeks to "constrain growth in the use of the private car by controlling the amount of parking provided by developments" and offers guidance to the level of parking which should be provided on sites such as this in close proximity to public transport routes.
- To minimise private parking requirements, visitor car parking for commercial uses should be shared with other non-residential uses, especially when the peak time of the varying uses are complementary (i.e. shops which are primarily used through the daytime can share parking spaces with leisure and restaurant uses which are predominantly used in evening hours) to ensure the parking provisions provided are used with maximum efficiency.
- In any circumstance where a developer feels the needs of commercial premises can be adequately catered for through a lower provision of car parking spaces than advocated in PPG13, the onus will be on the developer to demonstrate their proposal provides adequate parking provision through the use of a robust travel plan to be agreed by the City Council
- In general, visitor parking associated with any commercial/educational uses should be consolidated into shared car parking facilities to minimise the need for numerous separate car parks across the masterplan area. Such facilities should be designed to high standards and located in suitable locations around the site. The use of s106/CIL should therefore be encouraged to bring forward delivery of communal parking facilities when development within the masterplan area meets a suitable threshold. Travel plans should also be used to identify any surplus parking provision in the area which could be used to support visitor parking requirements for any given planning application.
- All staff parking and service areas should be located within the building block to the rear of commercial properties and concealed from view from the public realm. This is to ensure the aesthetic quality and visual amenity of the wider development is not compromised and the impacts of noise associated with these uses are mitigated wherever possible
- In general all parking and access to commercial uses will be taken from secondary streets running through the area. The number of entrance points into parking and service areas will also need to be kept to a minimum to limit the visual impact such entrances will have on building lines
- Service areas and areas of private parking which have an overly intrusive or detrimental impact on the aesthetic quality of the masterplan will not be permitted
- In addition to the above, any proposed hotel uses will need to include a suitable provision of taxi and/or private vehicle drop-off bays



## Access and servicing

- Masterplan site boundary
- cWHS boundary
- Vehicular traffic route
- Vehicular 'access only' pedestrian priority route
- Metro route
- Potential ferry route
- Bus accessible route
- Cycle route
- Parking area
- Service area





The Cube Car Park, Sheffield

### Residential parking

- Parking provisions for residential uses should be provided in line with the maximum provisions proposed in PPG13: Transport according to their use, scale and location. This is in line with policy T23A of the councils UDP Alteration No. 2 which seeks to “constrain growth in the use of the private car by controlling the amount of parking provided by developments” and offers guidance to the level of parking which should be provided on sites such as this in close proximity to public transport routes.
- In any circumstance where a developer feels the needs of residents can be adequately catered for through a lower provision of car parking spaces, the onus will be on the developer to demonstrate their proposal provides adequate parking provision through the use of a robust travel plan to be agreed by the council. Both of these approaches are endorsed by policy included in PPG13: Transport

- In general, all allocated parking provision will be accommodated within the curtilage of building blocks and will be located behind the building line of primary building frontages. This is to protect the visual amenity and aesthetic quality of the public realm
- Areas of private parking which have an overly intrusive or have a detrimental impact on the aesthetic quality of the masterplan will not be permitted

### Public parking

- Currently containing a range of tourist destinations including the National Glass Centre and more importantly, the cWHS, there is currently, and will likely remain a demand for public parking within the masterplan area. However, in line with guidance above, this should primarily be accommodated through shared car parking facilities to ensure parking provision across the site is maximised and the need for numerous parking areas minimised.
- The use of s106/CIL should be encouraged to bring forward delivery of communal parking facilities when development within the masterplan area meets a suitable threshold. Such facilities should be designed to high standards and located in suitable locations around the site. Travel plans should also be used to identify any surplus parking provision in the area which could be used to support visitor parking requirements for any given planning application.
- Further details with regard to the provision and location of public parking in the area will be clarified by the council in due course
- New development proposals must identify opportunities for the installation of electronic vehicle charging infrastructure. The North East of England has secured funding under the ‘Plugged in Places’ Program.

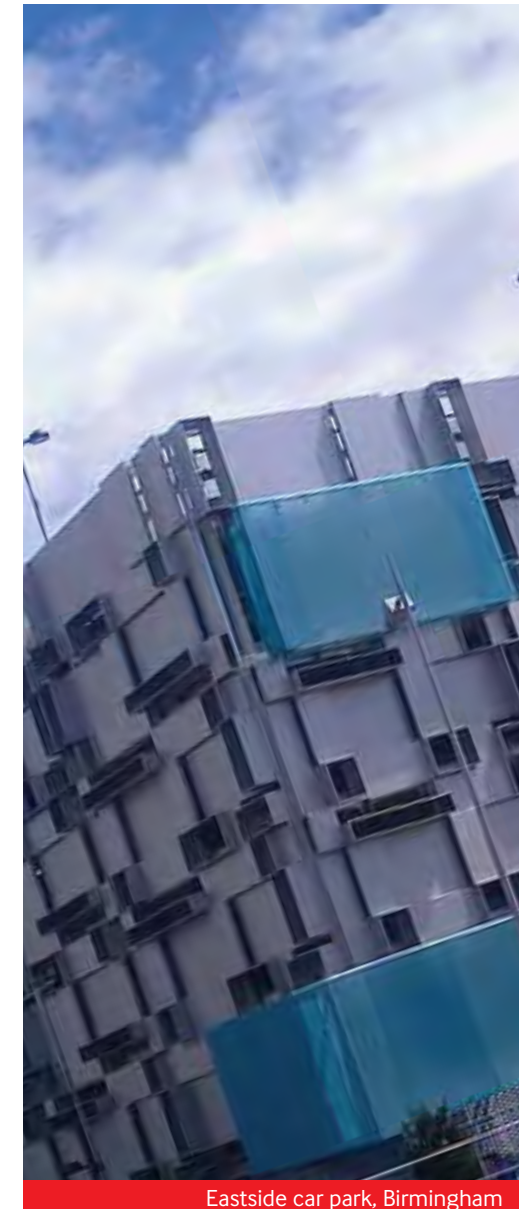


### Public transport

- Located within 1km of the city centre and immediately adjacent to the Tyne and Wear Metro and a range of bus routes, the masterplan area is already well served by public transport, a feature of the area which should be harnessed and fully utilised through new developments in the masterplan area.
- All primary routes around the masterplan area should be designed and built to a specification which allows them to retain their status as recognised bus routes to ensure the masterplan area reinforces the council's aspiration to promote a modal shift away from private vehicle use and towards sustainable forms of transport.
- Whilst the council does not have the resources available to guarantee the delivery or supply of additional bus routes to serve the masterplan area in the future, the council is keen to participate in discussion with Nexus (the local passenger transport executive) and prospective developers to ensure adequate provision of public transport is maintained and, where appropriate, expanded. Nexus was one of the stakeholders consulted at the development stages of the masterplan.
- Mechanisms available within the planning process, such as planning obligations through s106 or CIL, will be explored with prospective developers to enhance the provision of public transport to, from and through the masterplan area.

### Cycle Parking

- Where required, developments must provide an appropriate mix of short-stay/ long-stay cycle parking.
- Short-stay cycle parking must be located as close as possible to entrances of buildings, be secure and well overlooked from buildings or by passers-by. Long-stay cycle parking must be sited in a safe and well lit location which is accessible, convenient and overlooked.



Eastside car park, Birmingham





Oozle Square, Birmingham

### 9.10 Landscape and public realm

The landscape environment across the masterplan area will have a fundamental role to play in the character of the area. Similar to the ways in which streets are structured across the area, landscape, open space and public realm will be designed around a clear hierarchical structure of high quality and aesthetically pleasing environments.

Parks and open spaces will embrace recreation and leisure provision, harness the area's ecological and nature conservation value and will maintain and enhance the qualities of the public realm and green spaces. All areas of public realm will act as amenity space not just for the masterplan area but also for communities in the wider area, including local residents and visitors to the masterplan area. As a consequence, the design of such spaces should be as coherent and robust as possible to ensure the sustainability of the whole masterplan area.

The general codes covering areas of open public space across the masterplan area are as follows:

- The delivery of high quality open space across the site is of equal importance to the delivery of high quality built forms, and as a consequence all built developments to be delivered as part of the masterplan will be expected to contribute to the delivery of high quality open space both in and around the cWHS.
- Open space will support a range of leisure and recreation uses and should be designed to host and accommodate such pastimes to promote a healthy living environment for local residents
- The safety of open public space will be a key consideration when designing the public realm, with routes and spaces being overlooked by built development frontages providing natural surveillance and a positive relationship with the built form. Planting and street furniture should not be located so to block natural surveillance across open spaces
- Areas of land which are not suitable for development and do not compromise the deliverability of surrounding development sites (slopes, verges and cliffs etc) should be given over to wild/natural planting so to harness and support the areas local ecology.
- Existing planting and vegetation which displays significant ecological value will be retained as a matter of priority and incorporated into the masterplan and its wider landscaping proposal
- Materials must be relevant to the context of the area's surrounding built form, and planting should be indigenous to encourage habitat diversification and encourage biodiversity
- Respect for the area's context and natural ecology will be incorporated into the design of any open space. Regard must be had to the outcomes of the Strategic Environmental Assessment and the Appropriate Assessment report which accompany this SPD.

## Public realm

- Masterplan site boundary
- cWHS boundary
- St Peter's Square
- Feature hard landscaping
- Formal park land
- Feature landscaped spaces and nodes
- Existing open/green spaces
- Formal tree planting
- Wild/buffer tree planting
- Street tree planting
- Green links





## St Peter's Square

As the single most important space within the masterplan area, St Peter's Square will form the centrepiece for the area's public realm and provides a real opportunity to provide a truly unique and bespoke space showcasing one of the most important heritage assets in the region.

Taking into consideration the sensitivities associated with this space, following discussions with English Heritage, the council and its partners have taken the decision to commission a separate landscape vision for the site to support and be read in conjunction with this document. This will provide a better opportunity to deal with this important space in a greater level of detail and give a greater insight into the unique characteristics of the site. It will also allow the development of a more coherent and joined up approach between the St Peter's Wearmouth site, and it's sister site in Jarrow which together form the cWHS.

Given the importance of the space, and the need to ensure that any new development within the masterplan area benefits the setting of the cWHS, there will be a need and expectation for all new developments within the masterplan area to make a financial contribution to the delivery of the landscape vision. These contributions will be in addition to any other contributions associated with public realm improvements to individual

site's immediate surroundings and will be determined according to the scale and quantum of development proposed. This is to ensure the financial burden of delivering the landscape strategy is not unfairly focused on sites immediately adjacent to the cWHS and thus compromise the deliverability of regeneration sites next to St Peter's Square.

A detailed design brief is currently being developed by the council as part of the landscape vision and should be available to prospective developers in due course.



Peace Gardens, Sheffield

## Feature spaces and nodes

Creating a sequence of nodes and spaces through an area is a key component to landscape design when trying to create attractive and easy to use links and routes from one point to another. Providing well designed nodes and spaces at key points and junctions along a route can help significantly improve legibility, helping people orientate themselves and therefore increase permeability, and also helps create a range of intimate and vibrant spaces around which key activities and events can be focused.

In order to achieve this aspiration, the quality and design of spaces is paramount. Consequently, the following design code must be satisfied when designing such spaces:

- The design of spaces needs to be integral to the buildings and structures which surround them. Building entrances, corners, views and vistas all need to be suitably represented, celebrated and acknowledged in the design of spaces.
- The design of spaces needs to be unique and bespoke according to their setting and context in order to create a sense of place and character. Their design therefore needs to clearly demonstrate how they support and encourage their own unique roles within the masterplan area, accommodate surrounding desire lines, land uses and activities, and reduce the risk of monotony and repetition between spaces.



Brindley Place, Birmingham



- The topography of spaces should be addressed through imaginative design solutions and made features in their own right, celebrating the valley setting of the wider masterplan area. Opportunities should therefore be sought to celebrate changes in topography through the use of features such as steps, seating, planting and/or water features.
- The proportion and quality of hard and soft landscaping should be relevant and suitable to the needs of that space. A suitable provision and quality of hard landscaping needs to be provided so that envisaged pedestrian flows can be accommodated. This however needs to be balanced with a suitable provision of soft landscaping so spaces do not become sterile, harsh and unattractive.
- To support spaces as hubs of activity through both daylight and evening hours, opportunities to provide feature lighting should be explored. This will ensure the character of spaces will evolve throughout the day and that spaces remain active and vibrant throughout the evening.

### Hard landscaped public realm

Taking into consideration the pedestrian nature of large areas of the masterplan area, a majority of any new links or routes provided are likely to be in the form of pedestrianised or pedestrian priority streets. Consequently, achieving high design qualities through the hard landscaping which will make up these routes is imperative to achieving attractive and inviting public realm.

Hard landscaping should therefore be a design feature in its own right. It should be designed to the highest qualities, be sustainable, and provoke and encourage pedestrian movement across the site. As a result, the following design principles have been developed to ensure design qualities are delivered through proposals for the redevelopment of the masterplan area:

- The design and quality of the hard landscaped public realm across the masterplan area should be of a constantly high quality across the entire area. Proposals to upgrade areas of public realm to a standard below that set in this document will be resisted to ensure a low precedent is not set for subsequent public realm works.
- The design and quality of hard landscaped public realm should suitably relate and complement the design of any adjacent feature spaces and nodes. The design of any hard landscaped routes should merge seamlessly into that of any feature spaces to result in comprehensive and consistent design solutions.
- To ensure routes can evoke a relaxed and pleasant character, opportunities should be sought along any such routes to provide small scale break out spaces providing small groups of seating or landscaping for people to sit and relax. However, the scale and quantum of such spaces should not conflict with the role of feature nodes and spaces.
- The design of hard landscaped routes should be sustainable to ensure continued use of the space is made possible throughout daylight and evening hours, all year round. Designs should therefore be able to accommodate seating, outdoor eating and drinking associated with any adjacent retail units, organised exhibitions and local events and celebrations.



Broadgate Square, London

## Soft landscaping and park land

Whilst located within a largely urban environment, areas of soft landscaping and park land still play a fundamental role in the masterplan area. Green spaces can complement an urban setting by offering a greater range of amenity beyond that which can be offered by hard landscaped areas. They often provide a more relaxed environment, can help support local habitats, and can also provide suitable mechanisms to address issues with urban drainage.

The general codes covering areas of soft landscaping and park land across the masterplan area are as follows:

- The design of new park land will be integrated and relate to the design of buildings adjacent to the park, especially in situations where buildings front directly onto the park.
- The boundaries of parks will be treated in an informal naturalised way to better integrate the park with its surrounding uses and to encourage pedestrian movement into the park to increase levels of activity

- A hierarchy of paths will be provided throughout the park to encourage pedestrian use and to increase the legibility of the wider masterplan area. Wider paths should be focused along the spine of the park, with smaller informal paths leading off the spine to provide a variety of experiences for park users.
- The inclusion of appropriately high quality surface materials and street furniture is to be included within the design of the park. Materials should be relevant to their context and be durable.
- Appropriate lighting should also be provided along all routes to ensure users feel safe in both daytime and evening hours, although due regard must be paid to the potential impacts such lighting may have on surrounding habitats.
- Where new planting is proposed, attention must be paid to ensure the species are indigenous to the local area and are compatible with existing species to support and strengthen the area's biodiversity and existing habitats

## Wild/buffer planting

Having undergone an extensive site clearance program following the closure of the ship yards, the site is covered in a number of undeveloped sites, many of which are either grassed or being used as temporary car parks. Consequently, the ecological value of the area is limited, with the landscape quality of the sites being of low quality and often subject to tipping and vandalism.

Notwithstanding the above, there are some areas across the masterplan area which retain some ecological value in the form of a number of steep verges, embankments and cliffs which have been left to grow over time to form thick 'seams' of mature trees which run parallel to the river valley. Their location helps mitigate and protect some areas of the masterplan against noise from adjacent roads, and their scale and form also allows them to become an attractive and distinctive feature along the Wear gorge.

Due to the ecological and aesthetic qualities they bring to the area, any development within the masterplan area should seek to preserve and enhance this role. To support this approach and to ensure existing habitats are not compromised, the following codes have been developed:

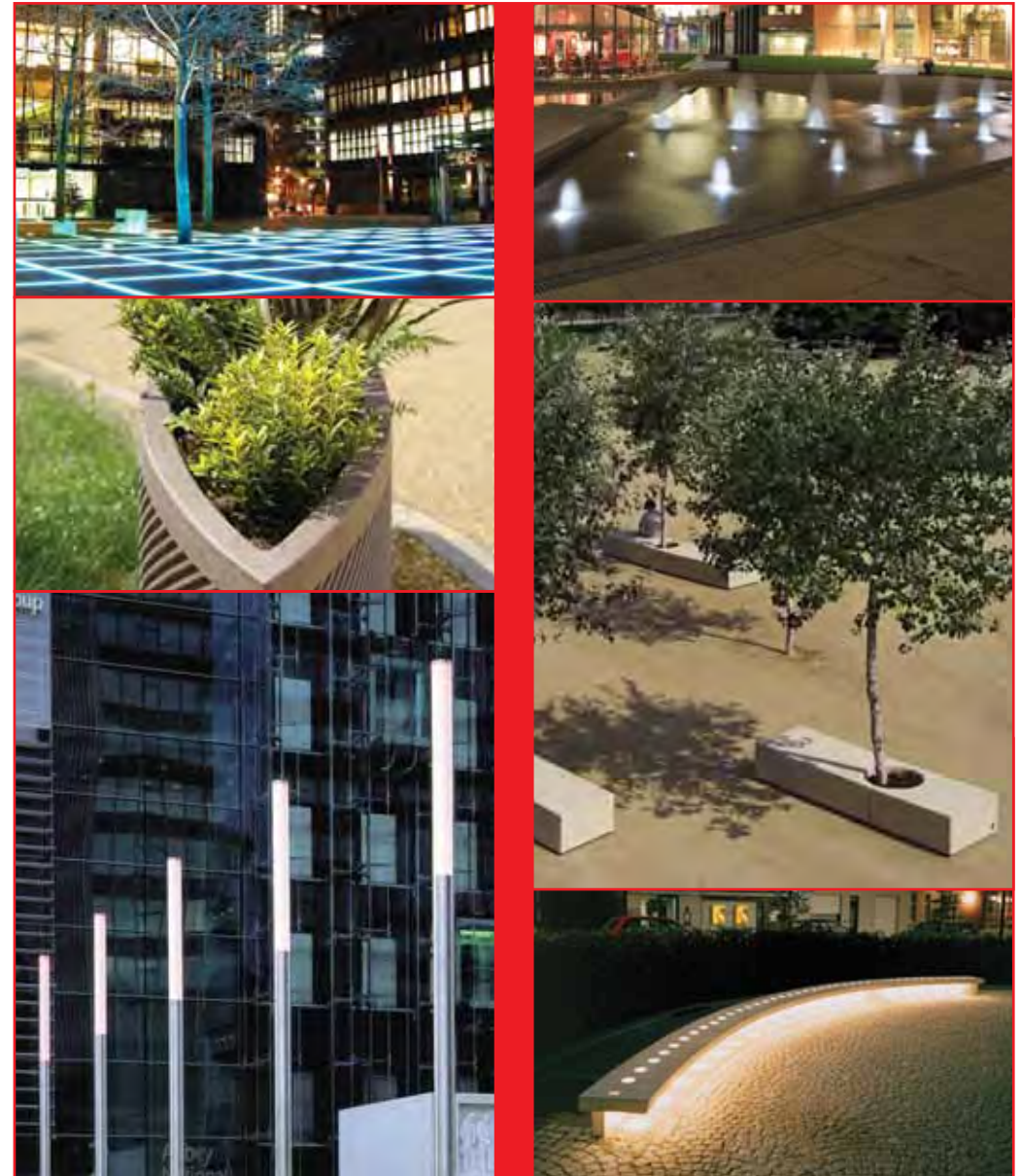
- All existing areas of vegetation as discussed above which show evidence of having biodiversity and/or ecological value will be retained and incorporated into the masterplan as a matter of priority
- Opportunities to improve the quality of existing vegetation through controlled pruning, planting, and management should be explored as a matter of priority, with the extension and expansion of existing habitats being a key concern
- Where new planting is identified as an appropriate solution to enhancing the ecological value of an area of existing vegetation, attention must be paid to ensure the species are indigenous to the local area and are compatible with existing species

### Street furniture, materials, and public art

The composition of areas of hard and soft public realm are crucial to reinforce route hierarchies, character areas and site legibility, so it is important that the materials and furniture used accentuate the overall sense of place and conform with the high quality of development expected from the masterplan.

The images opposite illustrate the standard of furniture expected to be provided and must accord with the following codes:

- Materials should be suitable, durable and appropriate for their location to ensure longevity and to reduce the need for maintenance. The location of the masterplan area means materials must be capable of withstanding harsh environments both in terms of climatic, environment and human influence
- Materials and furniture should be sourced from a sustainable process and be obtained through a reputable supplier so that items can be replaced if damaged or vandalised
- Materials should be in keeping with their context to ensure fluidity with surrounding areas and influences. An appropriate palette of materials will be supplied by the council and advocated through the planning processes.
- Opportunities to incorporate pieces of locally commissioned public art throughout the public realm will also be welcomed to help enforce the legibility of the scheme and identify key areas such as gateways and nodes
- Footpaths and areas of hard surfacing should be designed to be suitable for people with limited mobility, including wheelchair users, to ensure that the riverside is accessible to all. Consideration should be given to design and layout as well as materials.
- All new developments will be expected to incorporate a lighting scheme to frame and enhance the appearance of developments at night particularly at the key gateways and routes identified in this plan. A thorough approach to lighting may also assist in creating a sense of community safety and way finding.
- Due to the sensitive habitat in the area, considerable care must also be taken to minimise the impact of light pollution particularly on the inter tidal area. Consideration must also be given to minimising energy consumption.





# 10.0 Implementation

## Timescale

This SPD is intended to guide development of the St Peter’s, Bonnersfield and South Shore areas over a period of the next 10 to 15 years. However, it may need to be reviewed in due course to take account of any significant changes of circumstances and subsequent changes to national and local planning policy.

## Delivery

Successful implementation of this SPD will rely on a partnership approach between the local authority, developers, regeneration bodies, utilities companies, landowners and the community. Collaborative working will be central to delivering a vibrant and thriving area that enhances the significance and values of St Peter’s. It will also ensure that an appropriate balance is struck between conservation, sustainability and development.

Implementation of the strategy provided within this SPD will require significant public realm and accessibility enhancements. In particular, the Wearmouth-Jarrow Partnership will also seek to deliver improvements within the areas identified on the masterplan in its Landscape Vision: Public Realm Strategy on the north bank of the River Wear in association with relevant private land owners and developers.

Within privately owned sites across the SPD area, individual developers will be responsible for implementing works on their land in accordance with an approved scheme.

The delivery partners for the key projects within St Peter’s SPD area, together with the additional areas, are provided in the table below.

For the purposes of the above table, short-term is 0-5 years; medium-term is 6-10 years. The council will seek to work with all developers to ensure that development meets the requirements set out in this SPD and explores every opportunity to fully contribute to the protection and enhancement of the Outstanding Universal Value of the St Peter’s candidate World Heritage Site.

## Phasing

The council will require a comprehensive approach to the redevelopment of any sites within the SPD area. If development forms part of a wider scheme, a phasing plan must be submitted to and agreed with the council at the initial planning application stage. An important part of any phasing plan will be the timing of the delivery of any associated environmental and infrastructure improvements, including any associated mitigation.

Most of the development opportunities in the SPD area lie on the north shore at St Peter’s and Bonnersfield. Of these, the majority fall within the development aspirations and land ownership of the Sunderland University. The phasing of this development will be largely dependent on the university’s development programme, in turn dependent on the availability of funding, development partners and the creation of enterprise, business and learning partnerships to occupy development. However, the university’s masterplan should provide indicative phasing details. This may span the full timescale of the SPD.

Ref	Key Project	Primary Delivery Partner	Timescale
1	St Peter’s Church and Churchyard	Wearmouth-Jarrow Partnership	Short-term
2	University Masterplan area	University and private sector linked to the partnership/ enterprise and residential development	Medium-term and long-term
3	Bonnersfield	Private sector	Medium-term
4	Scotia Quay	Private sector	Medium-term
5	Numbers Garth	Private sector	Medium term
6	High Street East and West	Public sector and section 106 agreement contribution	Medium-term
7	Other areas	Private and public sectors with section 106 agreement contributions	Ongoing

The SPD provides flexibility for phased development at Bonnersfield which will enable the private land-owner to take account of prevailing economic conditions in bringing forward development proposals. However, the developer will be required to prepare an overall development proposal and indicative phasing plan at the outset to satisfy the council regarding matters of form, scale, and design concepts. Extensive remediation works are also required to facilitate this development which may impact on viability and therefore the timing of delivery.

On the south shore there is possibly less scope for comprehensive re-development, with limited development opportunities interspersed among existing developments. The timing of individual schemes will be heavily dependent on private-sector developer interest – council land ownership notwithstanding – and prevailing economic and market conditions.

As an interim measure pending the provision of a new pedestrian bridge crossing east of Wearmouth Bridge, the council will work with potential funders and operators to deliver a viable cross river ferry service and investigate the potential for such a service to be extended to other riverside development sites outside the SPD area.

## Funding

The strategy identified within this SPD will require significant resources to deliver the changes identified within St Peter's. Funding will need to be secured through a combination of the following:

- Public sector funding, investment and land acquisition
- Private sector investment
- Developer contributions from Community Infrastructure Levy and section 106 agreements (see below)

A range of activities can be supported through different sources of funding including environmental and accessibility improvements, together with enhancements to the area's important heritage including the cWHS and listed building. Delivery will require substantial investment.

Various Wearmouth-Jarrow Partnership members are helping to fund the management of the cWHS and associated facilities and attractions, including the initial phase of the masterplan identified in the Landscape Vision: Public Realm Strategy. They are responsible for co-ordinating bids for funding from various sources. However, additional funding is required in order to deliver the identified works. Whilst the current fiscal tightening sets a context for the likely availability of funding for projects within the SPD area, potential funding could be derived from a variety of sources. Potential sources are provided within the table below, including type of funding, details

of who can apply, the priorities and potential works that could be funded. It should be noted that the likelihood of funding actually being available through these sources for projects in the St Peter's SPD area has not been considered. Many sources of funding are 'funders of last resort', requiring significant match funding. Applications for funding are increasingly highly competitive.

The following table shows some potential sources of funding currently available, subject to eligibility and other criteria – this will need to be reviewed and updated throughout the timescale of the SPD.

Funding source	Who can apply	Priorities	Potential work to be funded
<b>Regeneration</b>			
Regional Growth Fund	Private bodies and public/private partnerships	A £1.4bn fund operating across England from 2011 to 2014. It supports projects and programmes that lever private sector investment creating economic growth and sustainable employment  <a href="http://www.bis.gov.uk/RGF">www.bis.gov.uk/RGF</a>	Private sector investment that triggers growth and jobs. Some basic infrastructure that triggers private sector led economic growth as part of a wider investment.  It will not support speculative developments that do not explicitly address long term growth.
Homes and Communities Agency	Local authorities and businesses	Projects which will assist in an area's regeneration.	Regeneration improvements which will assist in the transformation of the St Peter's Riverside, focused on housing
European Regional Development Fund (ERDF)	Legally constituted community and voluntary organisations, private sector organisations, local authority and other public organisations including Higher Education Institutions	Currently: 1.Enhancing and exploiting innovation. 2.Business growth and enterprise.  <a href="http://www.communities.gov.uk/erdf">www.communities.gov.uk/erdf</a>	Land acquisition and development of business premises.
Tax increment funding (to be confirmed)	N/A  A power to allow local authorities to borrow against predicted growth in their locally based business rates	N/A	Could provide funds for key infrastructure and other capital projects which will further support locally driven economic development and growth.
Natural England - Access to Nature Initiative	Local authorities, charitable and non-profit companies	Access to nature aims to improve the quality of places and increase the number and diversity of people inspired by and enjoying the natural environment.  <a href="http://www.naturalengland.org.uk/ourwork/enjoying/outdoorsforall/accesstonature/default.aspx">http://www.naturalengland.org.uk/ourwork/enjoying/outdoorsforall/accesstonature/default.aspx</a>	Increasing the number of people experiencing the natural environment around the cWHS
Big Lottery Fund - Awards for All - Changing Spaces Community Spaces	Applications welcome from a not-for-profit group	Potential funding for community, health and environmental projects.  <a href="http://www.biglotteryfund.org.uk/funding-uk">http://www.biglotteryfund.org.uk/funding-uk</a>	Community, health and environmental initiatives



Funding source	Who can apply	Priorities	Potential work to be funded
<b>Heritage</b>			
English Heritage	Local authority partnership schemes	Funding to historic buildings, monuments and designed landscape and schemes in conservation areas.  <a href="http://www.english-heritage.org.uk/server/show/nav.1117">http://www.english-heritage.org.uk/server/show/nav.1117</a>	Improvements to cWHS, listed buildings and buildings in the conservation area
Architectural heritage fund	Organisations with charitable status or partnerships between a local authority and a charity	Projects to buildings that are listed, scheduled or in a conservation area and of acknowledged historic merit.  <a href="http://www.ahfund.org.uk/">http://www.ahfund.org.uk/</a>	Increasing the number of people experiencing the natural environment around the cWHS
Heritage Lottery Fund	Local and Regional Groups	Priority of working together with groups to develop projects that will give people a better opportunity to experience, enjoy, care for and protect our heritage.  <a href="http://www.hlf.org.uk/english">http://www.hlf.org.uk/english</a>	As above
JESSICA - Joint European Support for Sustainable Investment in City	Local authorities	European grant which allows EU Structural Funds to be used to make repayable investments in projects forming part of a coherent plan for sustainable urban development.  <a href="http://ec.europa.eu/regional_policy/funds/2007/jji/jessica_en.htm">http://ec.europa.eu/regional_policy/funds/2007/jji/jessica_en.htm</a>	As above and potential funding for University projects
European Cultural Foundation	European Cultural Foundation	Innovative arts projects which illuminate cultural diversity and processes of change within European society  <a href="http://www.eurocult.org/">http://www.eurocult.org/</a>	Art schemes relating to the cWHS
Arts Council England	Individuals, arts organisations and other people who use the arts in their work	Grants are available for activities carried out over a set period and which engage people in England in arts activities  <a href="http://www.artscouncil.org.uk/funding">http://www.artscouncil.org.uk/funding</a>	Public art schemes within St Peter's
<b>Transport</b>			
Local Transport Plan	Local authorities through highways	Integrated Transport Block - used to fund non-maintenance transport schemes e.g. small roads projects, road safety schemes etc	Transport / access improvements
Nexus	Partnership working with the local authority.	Improvements to the Tyne and Wear Metro system.	Accessibility improvements to St Peter's Metro Station

## Compulsory purchase and land disposal

Most of the significant developable land in the SPD area is already in the ownership of prospective developers – including the university – or the council. On the south shore the North of England Civic Trust is also a significant land owner with an obvious interest in securing development appropriate to the historic context of the area. It is not anticipated that land assembly and ownership will be a significant issue or an impediment to appropriate development elsewhere in the area.

Where land ownership presents a constraint to the implementation of comprehensive development proposals the council will consider the use of its compulsory purchase powers to secure timely and appropriate development for the social, environmental or economic benefit of the area and the city as a whole.

The council will also seek to use its existing land ownership in the area to facilitate development and the implementation of public realm and other infrastructure improvements where appropriate. Any disposal of land by the council for development will be accompanied by a development brief setting out site specific requirements in addition to the guidance contained in this SPD.

## Planning applications

As the broad principles of development are already established through the council's adopted planning policy and this SPD, the council will encourage prospective developers and land-owners to engage in meaningful pre-application discussions prior to submitting full planning applications.

Notwithstanding the above, developers will be expected to provide robust evidence to justify development proposals and demonstrate compliance with adopted policy and guidance at both national and local levels.

Developers must refer to the latest Tyne and Wear Development Control guidelines on the requirements for the submission of planning applications and are advised to contact the council's Planning Implementation Team to discuss proposals (Tel: 0191 561 2549; email: [implementation@qsunderland.gov.uk](mailto:implementation@qsunderland.gov.uk)).

Developers must have particular regard to the following requirements, although this is not necessarily exhaustive:

## Design and access statement

In accordance with government guidance, a Design and Access Statement is required to accompany most new planning applications. The purpose of Design and Access Statements is to describe and illustrate the design principles behind a proposal and to explain how the proposed design solution has been informed by the key principles of good design. Design and Access Statements for development within the SPD area should set out the way in which a development will contribute towards meeting the requirements of current policy, frameworks and guidance, including this SPD. In particular, the Design and Access Statement should clearly demonstrate that the development proposal has had regard to the design principles set out in chapter 9 of this document. All Design and Access Statements should be prepared in line with the City Council's Design and Access Statement Supplementary Planning Document. Further advice can be provided by the City Council through pre-application discussions.

## Archaeological assessment

Archaeological desk-based assessments and evaluation are likely to be required for major proposals to ensure that any potential archaeological deposits are adequately identified, investigated and recorded. There will be a presumption in favour of preserving any nationally important archaeological remains in situ. The scope of works will be specified by the County Archaeologist prior to the submission of a planning application, including work to be undertaken before determination of the planning application where required. An archaeological excavation and/or watching brief may also be required by planning condition.

## Heritage statement

A Heritage Statement is likely to be required for any planning application that relates to or has an impact on the setting of the area's heritage assets (particularly the cWHS). This should consist of a written statement that includes a schedule of proposed works, an analysis of the significance of the archaeology, history and character of the building/structure, a justification for the works and their impact on the special character of the building or structure, its setting, the setting of adjacent listed buildings (if appropriate) and the setting of the conservation area (where relevant).

### Visual impact assessment

A Visual Impact Assessment may be required for major developments to assess the potential landscape and visual impacts associated with the development, to assess their significance and provide appropriate mitigation.

Development proposals must take full account of the key views highlighted in this SPD (see section 4.6 above and Appendix 4). The Visual Analysis that accompanies this SPD will need to be considered and the impact of proposed development on the key views that it identifies. The scope of the Visual Impact Assessment and the key views to be assessed should be agreed with Sunderland City Council.

### Environmental impact assessment (EIA)

An EIA may be required for some major development proposals if they are likely to cause significant environmental effects. The developer should obtain a screening opinion from the City Council to establish the need and scope for EIA at the earliest opportunity.

If EIA is required it will need to consider the predicted effects of the proposal on the cWHS, taking into account measures for avoiding, reducing and, wherever possible, remedying any potential significant adverse effects, and providing beneficial effects.

The EIA must include as a minimum the following elements:

- A description of the development comprising information on the site, design and size of the development.
- A description of the measures envisaged in order to avoid, reduce and, if possible, remedy significant adverse effects.
- The data required to identify and assess the main effects which the development is likely to have on the environment.
- An outline of the main alternatives studied by the applicant or appellant and an indication of the main reasons for his choice, taking into account the environmental effects.
- A non-technical summary of the information provided above.

Additional information may be required if it is considered necessary and reasonable to expect the applicant to compile it. Applicants should refer to the Department for Communities and Local Government's Environmental Impact Assessment: A guide to procedures (2000).

### Geotechnical report

Current and, especially, historical activity in the SPD area may have given rise to the contamination of soil and groundwater. Development within the area must address any contamination issues on the site concerned, prior to construction. A geotechnical report should be prepared as part of any development proposals to identify any potential contamination issues. The report should include recommendations for remediation or mitigation appropriate to the form of development to be implemented prior to the construction of new development.

### Ecology

Development proposals must protect and enhance local biodiversity, flora and fauna, including the creation of improved habitats. As well as measures to deliver ecological enhancements, proposals should include measures to:

- avoid negative ecological impacts, especially those that could be significant
- reduce negative impacts that cannot be avoided
- compensate for any remaining significant negative ecological impacts

All development proposals must be accompanied by an ecological impact assessment (EclA) which sets out the ecological issues and proposed mitigation measures. The EclA should be undertaken by a suitably qualified professional, in accordance with good practice guidelines ([www.ieem.net/ecia](http://www.ieem.net/ecia)).

Development should be implemented in accordance with the findings and recommendations of the EclA.



## Transport Assessment

All developments should be designed to improve and contribute to pedestrian and cycle links to and from the candidate World Heritage Site, the riverside, St Peter's Metro station and surrounding areas.

In accordance with UDP Alteration policy T2A, Transport Assessments should accompany planning applications for significant development proposals in the area. These should illustrate accessibility to the site by all modes, and provide details of measures to reduce reliance on travel by car and improve accessibility by public transport, walking and cycling. The Transport Assessment should also investigate the travel needs of specific developments and balance the likely demand for car parking provision with aims to encourage alternative modes of transport to the private car.

The impact of developments on the Strategic Road Network (SRN) will also be considered. If appropriate, proposals which create significant impact will be subject to consultation with the Highway's Agency.

In order to provide a comprehensive approach to sustainable transport, Travel Plans should also be prepared for all development proposals. These will include site specific modal split targets and set measurable objectives to be implemented.

## Flood risk and drainage

The SPD area is bisected by tidal reaches of the River Wear. Although the river is identified as a Flood Zone 3 this is constrained within bank through the SPD area and no properties are identified at risk.

The area is understood to be unlikely to suffer from groundwater flooding, but surface water flooding could result from severe localised storms (Sunderland Strategic Flood Risk Assessment, 2010)

A Flood Risk Assessment (FRA) and /or Drainage Impact Assessment (DIA) for all proposed development sites within the SPD area will be required. The FRA will identify the potential sources of flooding, from tidal, fluvial, groundwater and on-site drainage sources, review flooding history, obtain critical water levels and determine the influences on river hydraulics. The FRA will consider flooding both individually and in combination. The assessment also covers a number of other factors such as loss of floodplain storage and surface water drainage issues.

The DIA will provide a preliminary review of the potential options for drainage designs that may be utilised to avoid or mitigate flood risks and will consider the physical changes the proposed drainage measures may cause to the development and to the area drainage efficiency beyond the proposed development. DIA will invariably offer an outline design for an appropriate sustainable drainage system that will achieve betterment compared with the existing flood protection measures. Sustainable Urban Drainage Systems (SUDS) should be incorporated into new developments to alleviate surface water flooding. Overall, there should be no increase in flooding and no increased risks to people and property.

All development should maximise opportunities to further mitigate the current surface water drainage issues to the surrounding community, through incorporating storage or measures to reduce conveyance. Such techniques should be identified within development proposals at the earliest possible stage.

## Community Infrastructure Levy

Community Infrastructure Levy (CIL) is a planning charge introduced by government in 2010. It allows local authorities to raise funds from developers to secure the timely delivery of a wide range of infrastructure works needed as a result of development. The levy should be based on an up to date development plan and a charging schedule which forms part of the local development framework.

The council has not prepared a charging schedule at the time of writing this SPD. However, chargeable development (identified in the relevant planning permission) within the SPD area will be subject to any future CIL charging schedule and accompanying rate of charge adopted by the council.

## Planning obligations (section 106 agreements)

The introduction of CIL has not removed the potential need for site specific planning obligations to make development acceptable in planning terms. However, the scope of planning obligations is now more strictly controlled to ensure there is no over-lap with CIL, particularly in relation to the pooling of section 106 monies to fund infrastructure that should be funded by CIL.

The council will therefore assess each planning application to establish whether mitigation or other works are required which should properly be funded by the developer through a section 106 agreement in addition to any contribution required through CIL. The council will ensure that the following tests of a planning obligation are satisfied:

- necessary to make the development acceptable in planning terms
- directly related to the development; and
- fairly and reasonably related in scale and kind to the development

There may be site specific issues which create abnormal development costs, which together with required planning obligations may impact on the overall viability of proposed schemes. In such circumstances the council may be prepared to negotiate with developers regarding planning obligations, but only on the basis of 'open book' dialogue. The developer will be required to prepare and submit an independent Viability Assessment as the basis for such negotiation.

Where a developer is required to undertake highway works in association with their development, the planning permission will require them to enter into a highway agreement under section 278 Highways Act 1980 to carry out the works.

## Sustainable development

The City Council is committed to sustainable development promoted through the Sunderland Strategy and Economic Masterplan. Sunderland will become “a clean, green city with a strong culture of sustainability” and “an entrepreneurial University City at the heart of a low-carbon regional economy”.

All development must meet minimum Target Emission Reduction (TER) levels, prescribed by Building Regulations current at the time of the development.

Developments should supply 10 per cent of the site’s energy consumption from renewable sources located on site. If constraints mean that on-site renewables are not feasible at this level, any unfulfilled balance can be discharged if the development demonstrates an equivalent reduction in overall energy consumption, on top of the TER current at the time of development.

All non-residential development should aspire to achieve a minimum BREEAM excellent rating (or higher as dictated by future guidance or legislation) unless the developer can provide a full justification why this cannot be achieved, in which case a very good rating may be acceptable.

Residential development should achieve a minimum level 4 Code for Sustainable Homes rating (or higher as dictated by future guidance or legislation). In addition, where appropriate, development should seek to achieve a Building for Life silver standard, to be assessed by the council. For further details with regard to the Building for Life process visit [www.buildingforlife.org](http://www.buildingforlife.org) or [www.sunderland.gov.uk/urbandesign](http://www.sunderland.gov.uk/urbandesign).

Developers must undertake and provide evidence of feasibility work into the potential for on-site renewable energy and combined generation of heat, power and cooling (including on-site distribution networks). This evidence should include consideration of potential energy users and sources adjacent to a development site. If renewables or combined heat and power are not included in development proposals, applicants are required to provide justification for this. Proposals should investigate the potential for measures to reduce water consumption and incorporate rain water harvesting and recycling techniques.

Sustainable construction techniques, including the use of locally sourced materials and recycling of construction waste, should be used wherever possible.

Evidence should be provided within an overall Energy and Sustainability Statement, to be submitted alongside any planning application.

## Monitoring and Review

The SPD will be monitored, reviewed and updated to ensure that it is, and continues to be used as a positive planning tool that effectively helps to guide development over the next 15 years, in accordance with the vision, principles and design code set out within it. This will ensure that the SPD remains relevant and in accordance with development plan policy.

Many of the public realm and accessibility improvements set out within this SPD relate to works to be delivered in accordance with the draft cWHS Management Plan and Landscape Vision: Public Realm Strategy. It includes specific details of how these works will be monitored and reviewed and will need to be considered as part of any overall review.



## Appendix 1 - Other planning policy and guidance

In addition to those already specified in the body of the document, the following saved policies of the adopted Unitary Development Plan (UDP) and Alteration No.2 are also particularly relevant to the area covered by this Supplementary Planning Document. However, the list is not intended to be exhaustive and developers should satisfy themselves regarding the applicability of any UDP policies.

### UDP Alteration No.2

#### Sustainable urban design Policy B2A

The City Council will seek to secure the highest possible quality of built environment and the creation of desirable places to live, work, shop and visit. To achieve high standards of urban design all new development will be required to:

1. Reinforce or enhance the established (or proposed) urban character;
2. Respond to and reinforce the scale, form, massing and patterns of townscape development which make a positive contribution to the distinctive townscape and architectural qualities of the area;
3. Ensure the arrangement of buildings define the enclosure of the street, with street frontages as continuous as possible with the minimum of gaps between buildings;
4. Contribute to a safe and secure environment by providing surveillance for paths, streets and public spaces;
5. Integrate with the existing street pattern as appropriate and provide choice and convenience of movement for pedestrians and cyclists;
6. Ensure parking provisions is considered as an integral element of the design;

7. Developments will be required to conform with the council's Supplementary Planning Guidance including design criteria set out in S.P.G No 3 Residential Design. Specific guidance for the Central Area/ City Centre will be prepared in the form of a City Centre design strategy;
8. Be accompanied by a design statement for all significant forms of development, setting out the design principles of a proposed development;
9. Respect and enhance the best qualities of nearby properties and the locality and retain acceptable levels of privacy (including proposals for extensions to existing buildings);
10. Ensure Sustainable Urban Drainage techniques (SUDS) are incorporated into development proposals, unless it can be demonstrated that this is not feasible.

All new major developments will be encouraged;

1. To achieve 10% embedded energy supply from renewable sources, unless it can be demonstrated that this is not feasible; and
2. To achieve high energy efficiency and to minimise consumption so that they achieve BREEAM and eco-homes very good or excellent rating (to include the redevelopment of existing buildings).

#### Tall buildings B2B

Tall buildings will only be permitted where they:

1. Make a positive contribution to the character of the site and the wider area;
2. Form a positive relationship with the skyline and topography of the site and the surrounding area;
3. Will not detract from established views of important buildings, structures and landscape features;
4. Have a proper relationship with the street (or ground);
5. Avoid generating adverse climatic conditions, particularly wind and overshadowing.

#### Modal split T2A

The City Council will seek to promote journeys by public transport, on foot and by bicycle. In order to secure a shift towards non-car modes all major proposals will be required to include a transport assessment. In addition non-residential schemes will be required to commit to travel plans and will include:

1. Site specific mode split targets; and
2. The measures to be implemented to achieve such targets.

## River Wear T7A

The City Council will support proposals that seek to encourage the use of the River Wear for transport, in particular for leisure related trips.

Public transport orientated development – parking standards T23A

Within Central Sunderland, the following car parking standards shall apply:

### Residential:

- Developments lying within 400 metres of a Metro station - average 1 space per dwelling; and
- Developments lying between 400 and 800 metres of a Metro station - average 1.25 spaces per dwelling.

### Business (B1):

- Developments lying within 500 metres of a Metro station - 1 space per 50sqm. gross floor space.

The standards are maxima and are based on averages over the development site area. For mixed use development the relevant standard shall apply to each separate category of development. Conversions to residential within the City Centre will not be required to provide on site car parking.

## UDP

### Resources for the future

#### R1

The City Council will work towards environmentally sustainable development which meets the economic and social needs of the city. All proposals for development will be considered in relation to achieving a satisfactory balance between accommodating change and protecting valued and important aspects of the natural and built environment.

The impact of development will be considered against the need to respect the long term welfare of the environment by:

- making the most efficient use of land, energy and other resources
- reducing reliance on the use of the private motor car
- avoiding the risk of serious environmental damage, especially damage which may be irreversible or very difficult to undo.

#### R4

In assessing proposals for development (including changes of use), the council will have regard to evolving government policy on the efficient use of energy and use of non-fossil fuel sources. Development proposals will be encouraged which incorporate energy saving measures by virtue of grouping and orientation of buildings, the provision of shelter and energy saving technology. Combined heat and power schemes will be encouraged in major new developments and restructuring schemes.

### Economic development

#### EC8

The council will support the expansion of activities catering for tourists and other visitors by:

- identifying, consolidating and safeguarding attractions
- refusing proposals which would have an adverse impact on tourist attractions (as identified in the area chapters)
- actively encouraging opportunities for new tourist initiatives, especially where they are near existing areas of visitor interest
- providing cycle and car parking for visitors, and footpaths and interpretative facilities at tourist attractions

The environmental implications of any proposals, particularly in the defined coastal areas, will be taken into consideration.

### Housing

#### H21

Within new residential developments of more than 40 bedspaces:

(A) Amenity open space / casual playspace should be provided as follows:

- a minimum of 0.4 has per 1000 bedspaces where the site is within 0.5km of a neighbourhood (or larger) open space
- a minimum of 0.9 has per 1000 bedspaces where the site is not within 0.5km of a neighbourhood (or larger) open space

(B) Additionally, in the case of family dwelling, formal (equipped) children's playspace should be provided at a minimum of 0.2 has per 100 bedspaces.

In applying this policy, the council will take into consideration existing provision in the locality.

### Community facilities

#### CF2

Where land or buildings currently in community use become surplus to requirements, priority will be given to alternative public / community uses unless the site is identified for redevelopment in accordance with another proposal of the plan. Otherwise the provisions of policy EN10 would apply.

#### CF7

The City Council will assist the University of Sunderland to secure the implementation of its proposals to consolidate its educational activities onto two campuses (identified in part 2) and to provide an adequate level of students' residential accommodation to meet the needs identified in its development programme.

### Leisure

#### L12

The council will promote the recreational and tourist potential of the coast and riverside by improving access and encouraging development which provides for the need of visitors (including hotels), without adversely affecting the environment and conservation requirements.

## Built environment

### B4

All development within and adjacent to conservation areas will be required to preserve or enhance their character or appearance. To this end the council will issue planning / design guidance for the various areas from time to time.

### B6

The council will preserve and enhance the character or appearance of conservation areas; measures will include:

- encouraging the retention of existing buildings and the improvement of features, open spaces, historic street patterns and plot boundaries
- encouraging the retention of mature trees
- introducing controls over the display of advertisements
- seeking, where appropriate, to control development by the use of Article 4 Directions
- giving special attention to the preservation of important views into and of the area
- restoring highways and verges by use of appropriate materials and planting and encouraging utility companies to respect such works
- reducing the impact of traffic where possible by diversion and traffic calming measures
- promoting environmental improvement and enhancement programmes.

### B7

Applications for demolition of unlisted buildings in a conservation area will be determined by the extent to which the integrity, character and appearance of the area is affected, taking into account any replacement proposals. Where unlisted buildings make a positive contribution to the character or appearance of a conservation area, the criteria in policy B8 which concerns the demolition of listed buildings will apply.

### B8

There will be a presumption in favour of retaining listed buildings. Demolition in whole or substantive part will only be given consent when all other avenues for retention (including preservation in charitable or community ownership) have been explored and found not to be feasible or it is considered that redevelopment would produce substantial benefits for the community which would decisively outweigh the loss resulting from demolition. Consent will only then be given when planning permission for an acceptable replacement development has been granted, which will also be subject to conditions requiring the letting of a contract prior to demolition.

### B10

The City Council will seek to ensure that development proposals in the vicinity of listed buildings do not adversely affect their character or setting.

### B11

The City Council will promote measures to protect the archaeological heritage of Sunderland and ensure that any remains discovered will be either physically preserved or recorded.

### B15

Where major developments involve large scale ground disturbance in currently undeveloped areas, the City Council will determine whether, and to what extent, an archaeological assessment is required.

### B16

Where any historic sites and monuments are discovered provision will be made for an appropriate level of assessment, recording and preservation (in advance of or if necessary during construction) commensurate with the importance of the find.

### B17

The City Council will undertake and encourage schemes for the management, interpretation and promotion of important features, including:

- Listed buildings
- Ancient monuments
- Conservation Areas
- The urban riverside

Measures will include the provision of information boards and plaques, appropriate signposting and improvements to access.

### B19

All developments to which the public have access will normally be required to incorporate measures to:

- provide for ease of access and use by pedestrians generally and particularly for people with impaired mobility and those with pushchairs, shopping trolleys, etc.
- provide protection from inclement weather
- assist community safety and protection from crime

### B20

The City Council will undertake and encourage the provision of works of art, craft or decoration in major new developments and as part of the enhancement of the built environment and the open landscapes of the city.

## Countryside and nature conservation

### CN13

The City Council will protect and enhance important public views of townscape, landscape and other features of value (as identified in part 2), in particular as perceived from transport corridors and well used outdoor venues. New development should be located and designed so as not to unduly interrupt or prejudice views of recognized value; opportunities to enhance such views will also be taken into account when considering proposals.



#### CN18

The promotion of the interests of nature conservation will be sought throughout the city; the council will work together with neighbouring authorities and other agencies in regard to aspects affecting the wider area. Areas of nature conservation interest, particularly those of national importance, will be protected and enhanced; measures will include:

- the appropriate management of City Council owned land
- encouraging landowners and occupiers to adopt management regimes sympathetic to nature conservation, especially in wildlife corridors
- making provision in development proposals for preservation of habitats or creation of compensatory habitats
- seeking opportunities in development proposals or other schemes for new habitat creation on both public and private land
- improving access and providing interpretation to appropriate sites of wildlife interest
- refusing inappropriate development

#### Highways and traffic management

##### T14

Proposals for new development should:

- be readily accessible by pedestrians and cyclists as well as users of public and private transport from the localities which they are intended to serve
- not cause traffic congestion or highways safety problems on existing roads – where this criterion can not be met modifications to the highways concerned must be proposed to the satisfaction of the relevant highway authority and the cost of these must be met by the developer
- make appropriate safe provision for access and egress by vehicles (including buses), pedestrians, cyclists and other road users, paying particular attention to the needs of people with mobility impairment
- make provision for the loading and unloading of commercial vehicles
- indicate how parking requirements will be accommodated

##### T21

The provision of parking in the city will take account of the need to:

- maintain safe road conditions
- reduce travel demand to a sustainable level
- ensure the economic viability of existing retail and commercial centres
- promote more environmentally sensitive modes of transport

## Appendix 2 - Adopted viewshed analysis

Land within the buffer zone and wider setting of the Property at Wearmouth is undergoing major regeneration in ways which could impact on key views into and out from the Property. Development pressures are less significant at Jarrow. Some development within the setting of the Property presents valuable opportunities to enhance key views.

All new developments are subject to the Unitary Development Plan (UDP) and Local Development Framework (LDF) documents, both those already in place and those in development. These require design constraints to be applied to new developments to ensure that any new buildings within the setting of the Nominated Property provide a high standard of architecture that will serve to enhance its setting appropriately, as well as safeguarding the physical attributes of its Outstanding Universal Value.

The Commission for Architecture and the Built Environment (CABE) and English Heritage have produced guidance on Tall Buildings English Heritage have developed a methodology<sup>2</sup> to assess the impact of potential developments on historic sites, and have recently published guidance on the setting of heritage assets<sup>3</sup>.

The Conservation Plan (2010) identifies the key views into and out of the Property which impact on its Outstanding Universal Value and the attributes of Outstanding Universal Value, and hence should be protected and enhanced. These are illustrated and described on the following pages.

The following appraisal of viewsheds into and out of the Property analyses the key views for protection and enhancement in the context of the attributes of Outstanding Universal Value, as set out in the following box. Matters of a visual or aesthetic nature are of primary concern in relation to the first two attributes, which are referred to as the 'visual attributes':

### Wearmouth

The natural topography and meandering of the River Wear restricts some views. Views northwards across the river offer a wider field of view due to the gradually rising landform on the north banks of the river and the smaller scale development to be found there. Southwards, the topography is more dramatic. Large-scale developments within the City Centre prevent long-distance views in that direction. There are seasonal limitations to the extent of views within and outwith the buffer zone. For example, views to and from St Peter's Church are affected by the extent of trees surrounding its grounds.

### Key Views outwards from St Peter's monastic complex

From within the grounds of the Wearmouth monastic complex, short range views of the whole complex, including aboveground and marked out below-ground survivals, are to be enjoyed. These demonstrate the attributes of Outstanding Universal Value, and are protected through the designation of the whole site as a Scheduled Ancient Monument.

The most significant views available from within the confines of the monastic complex outwards are, in general, the converse of views identified as 'Key', as illustrated and described below. These are primarily of significance where they allow appreciation of the estuarine setting of the monastic complex. These views largely present the twentieth-century development that surrounds much of the perimeter of the complex. Some of these structures are of admittedly poor quality and appearance by virtue of their origins: the industrial development of the area when shipyards were the dominant land use (a consequence of its estuarine setting by an excellent natural harbour), followed by its twentieth-century social and economic history as industry declined.

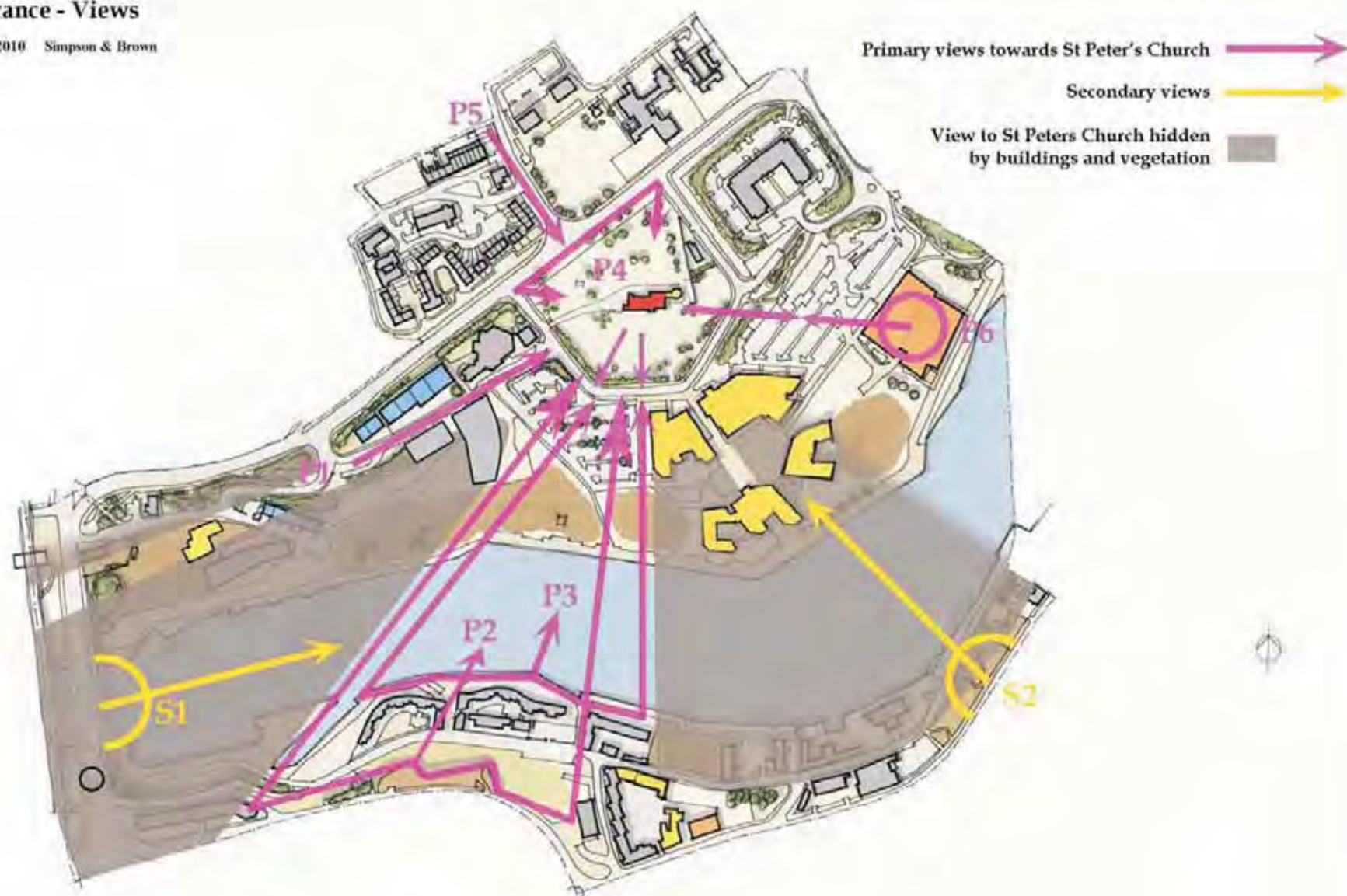
The known monastic complex was originally situated within a built context, surrounded by ancillary built structures. However, some elements of its current setting (for example, the industrial units along Charles Street) are not commensurate with the high quality of architecture originally associated with the World Heritage Property. Sunderland University and the National Glass Centre, which both feature the Property's immediate surroundings, reflect some of the Property's intangible values, in particular its ethos of learning and stimulating craftsmanship. Sympathetic and appropriate regeneration in the setting of the Property offers opportunities to protect and enhance the Property's attributes of Outstanding Universal Value. The emerging St Peter's Riverside and Bonnersfield SPD will set out how this will be achieved, including criteria which will guide the location, scale, massing, and architectural quality of new and replacement development to protect the Outstanding Universal Value and enhance the setting of the Nominated Property.

Two of the views outward from the Property, beyond the boundary fence, are considered to be 'key views', as described below.

# ST PETER'S CHURCH WEARMOUTH

## Setting Significance - Views

Not to Scale September 2010 Simpson & Brown





**Converse of P3: A view from the monastic complex through the University Campus towards the River Wear and across it to the viewpoint on the south shore.**

**Description:** The converse of view P3 allows the perception of the river gorge, although the topography prevents views of the river itself. In the foreground, the view is framed by the University Campus. Beyond the river gorge, the main element in the view is the City Centre skyline.

**Relevance to OUV:** Direct views south towards and across the river evidence the first of the attributes of OUV: the monastery's estuarine setting.

**Future expectations:** The surface car park taking up much of the foreground is proposed for redevelopment. The local planning authority's Development Management function, supported by the SPD, will ensure that any new development will protect and enhance the attributes of Outstanding Universal Value conveyed through this view. A substantial viewshed south towards and across the river will be retained, and a much needed enhancement of the Property's pedestrian connectivity with the riverside will be provided.

**Converse of P6: View from St Peter's monastic complex towards the National Glass Centre and beyond to the river corridor, harbour mouth and open sea.**

**Description:** The converse of P6, from the south-east sector of the monastic complex,

presents the National Glass Centre and, beyond it, the river mouth.

**Relevance to OUV:** This view admirably demonstrates the first of the attributes of Outstanding Universal Value: the estuarine setting of the monastery.

**Future expectations:** Enhancements in accordance with the 'Landscape Vision' are envisaged in order to open up the view, currently obstructed by the presence of trees and other landscape features. Enhancements to visitor orientation and interpretation are also planned, some of which will be implemented by April 2011, as this view also mirrors an important visitor route from the National Glass Centre to the Nominated Property.

**Key Views towards St Peter's monastic complex**

**P1 A contained view along Charles Street to St Peter's monastic complex.**

**Description:** This is a primary route for visitors approaching the site from the St Peter's Metro Station or the City Centre. The environs from which this view is enjoyed are poor, with many buildings of a poor built form and of low value, particularly the industrial units along Charles Street. However, pedestrian visitors can approach the Property with a direct view of the original west façade of St Peter's Church in front of them.

**Relevance to OUV:** This shows the Anglo-Saxon remains of the monastic complex with the seventh-century decorated façade of the west porch and tower as the main heritage assets in the view, with an unfolding view over the excavated area of the monastic complex, thereby demonstrating the second and third of the attributes of OUV.

**Future expectations:** This area is earmarked for future redevelopment provides the opportunity to improve the quality of the built environment and hence enhance the setting of the Property. Changes will be guided by the planning authority's Development Plan policies and controlled by its Development Management function. Shortterm improvements to signage and orientation will enhance the visitor experience.

**P2 An unfolding panoramic view over a length of some 300m at high level on the south shore with a particular vantage point from the current car park.**

**Description:** A high-level view of the monastic complex in its estuarine setting, over the River Wear, over the terraced landscape of the north shore reclaimed in the 1980's and 1990's from shipyard use, which forms the middle ground of this view. This area already possesses a viewing platform, although in poor condition. The amenity value of the environs from which these views are enjoyed is poor.

**Relevance to OUV:** P2 shows the whole known monastic site, including the monastic plan as demonstrated by in-situ archaeological remains, in its estuarine setting, with the Anglo-Saxon remains of the west porch and tower as the main heritage feature in the view. This demonstrates all visual attributes of Outstanding Universal Value.

**Future expectations:** This viewpoint is located in an area identified in the Sunnyside Planning Framework SPD as available for redevelopment. Proposals will be expected to incorporate a terrace from which views of the church and its estuarine setting will be preminent. The existing viewing platform could usefully be refurbished. The UDP supports reclamation of the terraced landscape of the north shore from shipyard use for redevelopment. Any building replacing the present surface car park will be expected to be set below the line of sight to the Property in this view. This view will be protected and enhanced through the planning

authority's Development Management function and the emerging SPD which will require any development to safeguard the visual relationship of the Property to the river.

**P3 An unfolding panoramic view at low level from the south shore over a length of some 250 metres with a particular vantage point to the east of the viewshed, aligned with a radial line of sight through the Campus to St Peter's monastic complex with the Anglo-Saxon west porch and tower as a focal point.**

**Description:** A view of the monastic complex from the south shore of the River Wear, with the University Campus and terraced landscape of the north shore, reclaimed from shipyard use, in the middle ground.

**Relevance to OUV:** P3 shows the whole of the monastic site in its estuarine setting, with the Anglo-Saxon west porch and tower as the main heritage features in the view. This demonstrates all visual attributes of Outstanding Universal Value.

**Future expectations:** The middle ground of this view on the north shore is identified in Sunderland City's adopted UDP as a 'Strategic Location for Change'. The terraced landscape of the north shore has been reclaimed from shipyard use for redevelopment. There are opportunities for enhancement whilst protecting and enhancing the OUV of the Property. Any development will be of a high architectural standard which enhances the setting and protects the Outstanding Universal Value of the monastic complex when viewed

from the south. This will be achieved through the local planning authority's Development Management function, supported by the SPD (see 4.3 above).

**P4 Close range views of St Peter's Church as observers move along Dame Dorothy St, with the Church suddenly revealed in landscaped setting.**

**Description:** Dame Dorothy Street forms a secondary route for visitors arriving to the monastic complex from St Peter's Metro Station. Tree and bush cover on the south side of Dame Dorothy Street both screen unattractive industrial units along Charles Street, and hide St Peter's Church and the surrounding monastic complex until the pedestrian is almost upon it. A cycle lane is provided along the pavement to the south of Dame Dorothy Street. To the east of the monastic complex, Dame Dorothy Street forms part of the Bede's Way footpath and Bede's Cycle Way that links the site with St Paul's, Jarrow. It also forms part of the main vehicular approach to the monastic complex.

**Relevance to OUV:** P4 shows the Anglo-Saxon west porch and tower, together with the marked out excavated monastic complex, as the main heritage assets in the view; it thereby demonstrates the context of the monastic site, facilitating an understanding of the attributes of Outstanding Universal Value relating to the monastic plan and in-situ archaeological remains both known and speculated.

**Future expectations:** The location of the Nominated Property at the hub of the modern university campus is very evident from this vantage point. The land surrounding the Property is identified as an urban park and will not change except to accommodate enhancements to improve movement through and about the Property.

**P5 A contained view along Church Street when approaching from the north.**

**Description:** This main approach route from the north for visitors arriving by private vehicle or bus to the monastic complex descends towards the Property allowing the monastic site's location, elevated above the river bank beyond, to be appreciated. The buildings which currently front Church Street and contain the view are of poor quality and detrimental to the amenity of this significant approach route.

**Relevance to OUV:** P5 shows the Anglo-Saxon west porch and tower as the main heritage asset in the view, with an unfolding view of the excavated monastic complex and river beyond. All of the attributes of Outstanding Universal Value are thereby evidenced.

**Future expectations:** This view south to the church, along a busy highway, is not expected to change significantly. It is hoped that the buildings which currently front Church Street will be redeveloped. Such redevelopment will be controlled and guided through the local planning authority's Development Management function and the St Peter's Riverside and Bonnersfield SPD which will set out criteria

to guide the scale, massing, location and architectural quality of new and replacement development which will serve to create a protected and enhanced setting for the Nominated Property.

**P6 A 360 degree panorama achieved from the roof terrace of the National Glass Centre showing the relationship of the river corridor and St Peter's monastic complex to the harbour mouth and sea beyond**

**Description:** The broad panorama visible from this key viewpoint illustrates excellently the intimate relationship of St Peter's monastic complex with the River Wear and, beyond, ready access to the sea. It gives a sense of how the monastic site may have been first experienced by travellers by boat reaching the safe waters of the estuary, and docking in a natural harbour below the monastic complex. As a publicly accessible viewing platform on a major visitor attraction, where interpretative material introduces visitors to the Wearmouth monastic complex, it is of particular value to the appreciation of the Property. However, tree cover around the boundary fence of St Peter's churchyard impedes views seasonally.

**Relevance to OUV:** Visible through the trees, the Anglo-Saxon tower is the main heritage assets in the view, with the site of the excavated monastic complex to its left hand side. The riverside to the east of the National Glass Centre formed an excellent natural harbour – an arrival point to the monastery. The view hence allows ready appreciation of the estuarine setting of the monastery. Hence, all attributes of OUV are demonstrated.

**Future expectations:** This viewpoint will remain largely unaltered. Views towards the Property are obscured at certain times of year due to the presence of tree belts to the south side of the churchyard and on the University car parks. These tree belts will be thinned out to open up views that will enhance appreciation of the Property.

### Other significant views at Wearmouth

A set of secondary views has been identified. These do not contain the Property or directly demonstrate visual attributes of its Outstanding Universal Value. They do, however, illustrate the dramatic geography of the Property's estuarine setting.

#### S1 Wearmouth Bridge

Description: This high-level 180-degree panoramic view of the river corridor extends to the mouth of the river and the sea beyond, demonstrating the geography of the estuarine site.

#### S2 Nobles Quay

Description: This low-level 180-degree panoramic view of the river corridor from the Wearmouth Bridge on the left, to the harbour mouth on the right demonstrates the geography of the estuarine setting. Wearmouth Bridge, one of the City's primary heritage assets, is seen to best effect from this vantage point. It is also an excellent vantage point from which to observe and monitor the effect of development over time on the appearance of the riverside and as such will be safeguarded in any redevelopment.

As at Wearmouth, natural topography restricts views in places. The viewshed from the public realm is generally well contained, apart from occasional glimpses of St Paul's tower from the south and the Rivers Don and Tyne. The generally flat landscape does not offer many elevated views and the surrounding tree cover provides a further limitation. The best archetypal viewpoint for the St Paul's monastic complex is from the bank of the River Don directly to the east of the monastery, which reveals the church at a high point in the landscape and reflects an original main arrival point to the monastic complex. The scale, compact form, modest tower, and surrounding landform all contribute to the discreet presence of St Paul's in its landscape setting.



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